

EXECUTIVE DECISION DAY NOTICE

Leader and Executive Member for Hampshire 2050 and Corporate Services & Deputy Leader and Executive Member for Hampshire 2050 and Corporate Services Decision Days

Date and Time Thursday, 9th March, 2023 at 2.00 pm

Place Remote Decision Day

Enquiries to members.services@hants.gov.uk

Carolyn Williamson FCPFA
Chief Executive
The Castle, Winchester SO23 8UJ

FILMING AND BROADCAST NOTIFICATION

This decision day is being held remotely and will be recorded and broadcast live via the County Council's website.

AGENDA

Leader and Executive Member for Hampshire 2050 and Corporate Services

Deputations

To receive any deputations notified under Standing Order 12.

KEY DECISIONS (NON-EXEMPT/NON-CONFIDENTIAL)

1. **COUNTYWIDE MOVING TRAFFIC ENFORCEMENT POLICY** (Pages 5 - 34)

To consider a report of the Director of Hampshire 2050 seeking approval for the adoption of the Moving Traffic Enforcement Policy which provides the policy framework and guiding principles for enforcing moving traffic offences.

NON KEY DECISIONS (NON-EXEMPT/NON-CONFIDENTIAL)

2. **ANDOVER ROAD WINCHESTER STRATEGY** (Pages 35 - 44)

To consider a report of the Director of Hampshire 2050 regarding the Andover Road Winchester Corridor Strategy.

3. FLEET AND SURROUNDING AREAS TRANSPORT SCHEMES UPDATE (Pages 45 - 54)

To consider a report of the Director of Hampshire 2050 on local transport priorities in Fleet and the surrounding area.

4. RUSHMOOR LOCAL CYCLING AND WALKING INFRASTRUCTURE PLAN (Pages 55 - 62)

To consider a report of the Director of Hampshire 2050 seeking approval of the Local Cycling and Walking Infrastructure Plan (LCWIP) for Rushmoor.

5. BASINGSTOKE LOCAL CYCLING AND WALKING INFRASTRUCTURE PLAN (Pages 63 - 72)

To consider a report of the Director of Hampshire 2050 seeking approval of the Basingstoke and Deane Local Walking and Cycling Infrastructure Plan and the next steps for future study work and scheme delivery.

6. WELBORNE STREET MANUAL (Pages 73 - 220)

To consider a report of the Director of Hampshire 2050 regarding the Welborne Street Manual.

7. MEMBERSHIP OF ASSEMBLY OF EUROPEAN REGIONS (Pages 221 - 228)

To consider a report of the Director of Hampshire 2050 regarding the County Council's membership of the Assembly of European Regions.

8. THE LEADER'S COMMUNITY GRANTS AND INVESTING IN HAMPSHIRE FUND (Pages 229 - 242)

To consider a report of the Director of Hampshire 2050 regarding applications for the Leader's Community Grant Fund 2022/23 and an award from the Investing in Hampshire Fund.

9. PARISH AND TOWN COUNCIL INVESTMENT FUND - APPROVAL OF LARGER GRANTS (Pages 243 - 248)

To consider a report of the Director of Hampshire 2050 regarding applications for the Parish and Town Council Investment Fund.

10. CHIEF OFFICER DELEGATIONS REGISTER (Pages 249 - 254)

To consider a report from Chief Officers seeking approval to redelegate existing Executive Member authority in favour of the Director of Economy, Transport and Environment to the Director of Universal Services.

EXCLUSION OF PRESS AND PUBLIC

RECOMMENDATION:

That the public be excluded from the meeting during the following item of business, as it is likely, in view of the nature of the business to be transacted or the nature of the proceedings, that if members of the public were present during this item there would be disclosure to them of exempt information within Paragraph 3 of Part 1 of Schedule 12A to the Local Government Act 1972, and further that in all the circumstances of the case, the public interest in maintaining the exemption outweighs the public interest in disclosing the information, for the reasons set out in the report.

KEY DECISIONS (EXEMPT/CONFIDENTIAL)

11. CORPORATE OFFICE ACCOMMODATION STRATEGY (Pages 255 - 270)

To consider an exempt report of the Director of Hampshire 2050 regarding the County Council's office accommodation portfolio.

NON KEY DECISIONS (EXEMPT/CONFIDENTIAL)

None

ABOUT THIS AGENDA:

On request, this agenda can be provided in alternative versions (such as large print, Braille or audio) and in alternative languages.

ABOUT THIS SESSION:

The press and public are welcome to observe the public sessions of the decision day via the webcast.

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HAMPSHIRE COUNTY COUNCIL

Decision Report

Decision Maker:	Leader and Executive Member for Hampshire 2050 and Corporate Services
Date:	9 March 2023
Title:	Countywide Moving Traffic Enforcement Policy
Report From:	Director of Hampshire 2050

Contact name: Dominic McGrath

Email: dominic.mcgrath@hants.gov.uk

Purpose of this Report

1. The purpose of this report is to seek approval for the adoption of the Moving Traffic Enforcement Policy which provides the policy framework and guiding principles for enforcing moving traffic offences.

Recommendations

2. That the Leader and Executive Member for Hampshire 2050 and Corporate Services approves the Moving Traffic Enforcement Policy attached to the report in Appendix A and its adoption.
3. That the Leader and Executive Member for Hampshire 2050 and Corporate Services delegates authority to the Director of Hampshire 2050 to make any updates and minor amendments as considered appropriate to the Moving Traffic Enforcement Policy in consultation with the Leader and Executive Member for Hampshire 2050 and Corporate Services.

Executive Summary

4. This paper seeks approval and adoption of the Moving Traffic Enforcement Policy document which provides the policy framework and guiding principles for enforcing moving traffic offences. This will be read in conjunction with a Code of Practice and guidelines on enforcement. An existing Code of Practice on Bus Priority and Traffic Enforcement Cameras was approved at the former Executive Lead Member for Economy, Transport and Environment Decision Day on 28 October 2021 (which, under existing delegated authority, will be updated to reflect the new moving traffic enforcement powers). Guidance notes on enforcement and cancellation of Penalty Charge Notices are included in an annex to the policy.
5. The policy and supporting documents will ensure there is a clear and consistent policy position and approach to the enforcement of moving traffic restrictions once all other reasonable steps have been taken to improve compliance.

Contextual Information

6. Approval was given at the Executive Lead Member for Economy, Transport, and Environment Decision Day on 27 January 2022 to the principle of the County Council undertaking civil enforcement of moving traffic restrictions and, after a successful application to the Secretary of State, these powers were granted to Hampshire County Council as part of The Civil Enforcement of Moving Traffic Contraventions Designations and Miscellaneous Amendments Order 2022 on 14 July 2022.
7. The Moving Traffic Enforcement Policy is written in accordance with the Department for Transport's most recent Statutory Guidance (The Traffic Management Act 2004: Statutory Guidance for Local Authorities outside of London on Civil Enforcement of Bus Lanes and Moving Traffic Contraventions).
8. The Moving Traffic Enforcement Policy sets out the context for moving traffic enforcement, the outcomes expected and how moving traffic enforcement will be implemented, delivered, and operated. The governing principles for moving traffic enforcement are:
 - Principle 1 Improving Road Safety

Enforcement of moving traffic restrictions at locations where there have been a history of collisions resulting in road users being injured or where there is risk to pedestrians within bus priority measures
 - Principle 2 Managing the transport network to ensure the expeditious movement of traffic

Enforcement of moving traffic restrictions at locations where congestion of the network regularly occurs or where contraventions within bus priority measures hinders the efficient movement of buses.
 - Principle 3 Improving the quality and accessibility of public transport

Enforcement of bus priority measures to assist with the safe and efficient movement of buses, raising the profile and attractiveness of public transport.
 - Principle 4 Improving the local environment and assisting with carbon reduction

Enforcement of restrictions aimed at easing congestion and reducing delays to emergency services.

Enforcement of restrictions that help improve air and noise quality and encourage more cycling and walking.
9. The County Council aims to use the new powers to address highway contraventions where those contraventions are known to either compromise highway safety, cause active travel users to feel uncomfortable using highway infrastructure, cause congestion of the network or result in delays to buses. This

will assist the County Council in fulfilling its Network Management Duty and its commitment to improve road safety, encourage modal shift and traffic reduction, reduce traffic congestion and improve air and noise quality.

10. The policy is essentially an enabling measure and all future schemes for enforcement, including necessary Orders, will be subject to individual assessment including business cases and financial implications. Where appropriate, some of these will be subject to future formal Executive Member decisions, particularly where there are significant financial implications or where there are major local sensitivities.
11. Delegated authority is sought to allow the Director of Hampshire 2050 to make minor changes to the policy in the future, this would allow the policy to be kept updated in response to any minor changes in legislation or national guidance. All revisions would be made in consultation with the Executive Member and a decision for anything of significance would be sought through the normal decision making process.
12. There are over 1,000 moving traffic restrictions and 42 bus priority measures of several types in force across the county that can be enforced under the new regulations. These include bus lanes, banned turns, yellow box markings, one-way streets, access restrictions such as prohibition of driving and bus gates, mandatory cycle lanes and School Keep Clear markings.
13. As part of its Traffic Management Duty, the County Council will regularly appraise accident records and arrange traffic surveys to identify where moving traffic offences are occurring. Where contraventions are identified, all reasonable steps will be taken to improve compliance by ensuring the restriction is clear and visible to road users. This will include the review and enhancement of traffic signs, and road markings and site lines are kept clear through regularly maintained vegetation. Camera enforcement schemes will only be taken forward where road users continue to fail to comply with the restrictions, despite these enhancements.
14. An initial ten locations, where current highway restrictions are subject to regular contraventions, are proposed in the first tranche of enforcement schemes. The sites have been identified on the basis of road safety records or where congestion of the network is known to occur or where traffic surveys identify regular contraventions that can hinder the safe and efficient movement of buses.
15. Further enforcement schemes may need to follow this initial tranche where an established case or policy need has been identified, and once all other reasonable steps have been taken to improve compliance.
16. Before any enforcement sites are commissioned, the County Council will arrange a 6-week on-line public consultation accessible from the County Councils website. The consultation will provide details on why the site requires enforcement and will provide the opportunity for comments and suggestions. The consultation will be publicised through Corporate Communications press and media channels.

17. Enforcement will be carried out using specialist approved traffic enforcement cameras that utilise ANPR technology. The County Council will manage the detailed design, procurement, supply, installation, and commissioning of ANPR enforcement cameras, associated signage and road markings.
18. To promote compliance by helping road users understand the seriousness of moving traffic contraventions, for a period of six months following implementation of the enforcement scheme, warning notices for first-time moving traffic contraventions will normally be issued instead of fines. This is a requirement of the Department for Transport's moving Traffic Enforcement Statutory Guidance document. The warning notice will set out the six-month period and advise that any further moving traffic contravention at the same camera location would result in the issue of a Penalty Charge Notice (PCN).
19. All enforcement measures will be kept under review on a regular basis. It is anticipated that compliance within moving traffic restrictions will improve over time.
20. To complement the Moving Traffic Enforcement Policy, a Code of Practice and guidance notes for the operation of bus lane and moving traffic enforcement set out the rules by which the system will be operated. This will ensure privacy, fairness and integrity issues are managed in accordance with legislation.

Finance

21. It is intended that enforcement of moving traffic restrictions will be cost neutral over time, with the full annual costs of operating and maintaining individual enforcement locations met from Penalty Charge Notices (PCNs). An early years revenue budget is likely to be required to meet any initial in-year deficit, for start-up and initial operating costs as sites are introduced countywide pending receipt of income from fines. Any investment and cashflow revenue funding will be subject to business case approval and in accordance with current standing financial orders and authorisations.
22. The requirement to issue warning notices, rather than PCNs, to first-time contraventions at each site during the first 6 months of enforcement will mean that the County Council will receive very little scheme revenue during this period. Budget provision will therefore need to be made to cover the back-office processing and administrative costs associated with the issuing of warning notices and will be funded through the Parking Services contract with the service provider NSL.
23. Fines for moving traffic violations have been set by the Government at £70 within the new regulations with a reduction to £35 if paid within 21 days. The rates are defined by Government and may change in future in line with national changes.
24. As with income from civil on-street parking and bus lane enforcement, any surplus revenue generated through fines from moving traffic enforcement, should it arise, must be spent in accordance with Section 55 of the Road Traffic Regulation Act 1984. In accordance with previous cabinet and council reports relating to the SP23 opportunity to offset costs for highways and transport delivery, once associated operational and set up costs have been covered, any surplus revenue should be prioritised towards other enforcement or traffic

management schemes, measures to improve public transport, active travel measures, or other associated highway improvement measures, including asset improvement, adaption and maintenance.

25. All future schemes introduced under the policy will be subject to their own financial assessment as part of the decision making process.

Consultation and Equalities

26. An engagement exercise was undertaken before the County Council's application for moving traffic enforcement powers. The engagement was open from 14 February 2022 and closed on 27 March 2022, it was an open engagement, so respondents were self-selecting.
27. Engagement was communicated through a range of channels including social media, direct communication to stakeholders and a press release to local media. Residents directly surrounding the proposed enforcement locations also had a letter hand delivered to their property.
28. A total of 432 responses were received with the majority of those who responded being in favour of Civil Enforcement.
29. A six-week public consultation will also be required for all future proposed Moving Traffic Enforcement sites.
30. Enforcement of the existing highway traffic restrictions will not affect the fundamental provision of the services and will have a positive impact on groups with protected characteristics. There would generally be benefits to the local community and, potentially, benefits for road safety from greater compliance. Enforcement of some Orders would, for example, potentially improve the experience of people using public transport, which would have a beneficial impact for a number of people with protected characteristics, who more commonly use buses and trains than the general population.
31. The equalities impact of individual schemes covered by the policy would be subject to EqIAs when they come forward in future, where the impacts could be more specifically assessed.

Climate Change Impact Assessments

32. Hampshire County Council utilises two decision-making tools to assess the carbon emissions and resilience of its projects and decisions. These tools provide a clear, robust, and transparent way of assessing how projects, policies and initiatives contribute towards the County Council's climate change targets of being carbon neutral and resilient to the impacts of a 2°C temperature rise by 2050. This process ensures that climate change considerations are built into everything the Authority does.
33. The policy will have a beneficial impact in terms of carbon reduction where it is able to support reductions in congestion or traffic, although the impact will be relatively small compared to the impact of other measures to reduce traffic and travel demand.

Climate Change Adaptation

34. A Climate Change Impact assessment will be carried out for each individual moving traffic scheme as it is developed.
35. Cameras installed on the highway network will be vulnerable to the extremes. The tool highlighted the greatest vulnerability to be via extreme storms which may affect cameras installed on site and network connections (via mobile signals) to central offices.
36. The project will ensure:
 - a requirement for the cameras to be Vehicle Certification Agency approved; and
 - sites will be assessed for storm, floodwater or coastal vulnerability concerns.

Carbon Mitigation

37. A full Carbon Mitigation impact assessment will be carried out for each moving traffic enforcement scheme. By reducing congestion and delays caused by the contravention of traffic restrictions, the proposal to enforce such restrictions will have a positive impact on carbon reduction. Congestion or collisions caused by inconsiderate or unsafe driving results in increased vehicle emissions.

REQUIRED CORPORATE AND LEGAL INFORMATION:

Links to the Strategic Plan

Hampshire maintains strong and sustainable economic growth and prosperity:	yes
People in Hampshire live safe, healthy and independent lives:	yes
People in Hampshire enjoy a rich and diverse environment:	yes
People in Hampshire enjoy being part of strong, inclusive communities:	yes

Other Significant Links

Links to previous Member decisions:	
<u>Title</u> Moving Traffic Enforcement Powers Moving Traffic Enforcement Powers-2022-01-27-ELMETE Decision Day (hants.gov.uk) Bus Priority Enforcement Policy Bus Priority Enforcement Policy and Practice - Appendix 1-2021-10-28-ELMETE Decision Day (hants.gov.uk)	<u>Date</u> 27 January 2022 28 October 2021
Direct links to specific legislation or Government Directives	
<u>Title</u> The Civil Enforcement of Moving Traffic Contraventions Designations and Miscellaneous Amendments Order 2022 (legislation.gov.uk)	<u>Date</u> 14 th July 2022

Section 100 D - Local Government Act 1972 - background documents	
<p>The following documents discuss facts or matters on which this report, or an important part of it, is based and have been relied upon to a material extent in the preparation of this report. (NB: the list excludes published works and any documents which disclose exempt or confidential information as defined in the Act.)</p>	
<u>Document</u>	<u>Location</u>
None	

EQUALITIES IMPACT ASSESSMENT:

1 Equality Duty

The County Council has a duty under Section 149 of the Equality Act 2010 ('the Act') to have due regard in the exercise of its functions to the need to:

- Eliminate discrimination, harassment and victimisation and any other conduct prohibited by or under the Act with regard to the protected characteristics as set out in section 4 of the Act (age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation);
- Advance equality of opportunity between persons who share a relevant protected characteristic within section 149(7) of the Act (age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation) and those who do not share it;
- Foster good relations between persons who share a relevant protected characteristic within section 149(7) of the Act (see above) and persons who do not share it.

Due regard in this context involves having due regard in particular to:

- The need to remove or minimise disadvantages suffered by persons sharing a relevant characteristic connected to that characteristic;
- Take steps to meet the needs of persons sharing a relevant protected characteristic different from the needs of persons who do not share it;
- Encourage persons sharing a relevant protected characteristic to participate in public life or in any other activity which participation by such persons is disproportionately low.

2 Equalities Impact Assessment:

Enforcement of the existing highway traffic restrictions will not affect the fundamental provision of the services and will have a positive impact on groups with protected characteristics. There would generally be benefits to the local community and, potentially, benefits for road safety from greater compliance. Enforcement of some Orders would, for example, potentially improve the experience of people using public transport, which would have a beneficial impact for a number of people with protected characteristics, who more commonly use buses and trains than the general population.

The equalities impact of individual schemes covered by the policy would be subject to EqlAs when they come forward in future, where the impacts could be more specifically assessed.

**MOVING TRAFFIC ENFORCEMENT
POLICY**

January 2023

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Introduction to Moving Traffic Enforcement Policy

This policy document sets out how the County Council will manage the civil enforcement of moving traffic restrictions and is written in accordance with Statutory Guidance issued by the Secretary of State for Transport under Section 87 of the Traffic Management Act (TMA) 2004. The TMA sets out the policy framework for bus lane and moving traffic enforcement, including how to approach carry out and review enforcement.

This Moving Traffic Enforcement Policy is one of a series of policy documents pertaining to civil enforcement and should be read in conjunction with:

- Code of Practice for the Operation of Bus Lane and Moving Traffic Restrictions, which sets out the objectives and the rules by which the system will be operated. This will ensure privacy, fairness and integrity issues are managed in accordance with relevant guidance and legislation.
- Bus Priority Enforcement Policy which sets out the guidelines and principles for enforcement at bus lanes, bus gates and other bus related highway restrictions (collectively called Bus Priority Measures)

This document will be reviewed on a regular basis to ensure that it continues to meet ongoing needs and is up to date in respect of legislation, regulations and guidance. Any changes to the policy shall be approved by the Executive Member for Hampshire 2050, or where minor in nature by the Director for 2050 in consultation with the Executive Member.

Background and Policy Objectives

There are over 1,000 moving traffic restrictions and 42 bus priority measures of several types in force across the County. These include banned turns, yellow box markings, one-way streets, access restrictions such as prohibition of driving and bus gates, mandatory cycle lanes, School Keep Clear markings and bus lanes. Contraventions within such measures can either compromise highway safety, cause congestion of the road network or impact on the safe and efficient movement of buses.

Managing the highway network is an integral part of the County Council's Network Management Duty under the Traffic Management Act 2004. This includes maintenance of highway assets, co-ordination of street works and events,

enforcement and managing traffic and congestion, ensuring the expeditious movement of traffic on the road network.

The County Council is committed to embedding a climate resilient integrated transport network that addresses the key challenges of congestion and environmental pollution in furtherance of a range of important outcomes as set out in national and local policy background.

This will be achieved by ensuring that, where there is a need to do so, existing or future bus priority measures and moving traffic restrictions are enforced, which will help achieve national and local objectives of:

- improving road safety
- improving the quality and accessibility of public transport
- improving the local environment, especially carbon reduction
- creating more pleasant places to live and work
- managing the traffic network to ensure efficient movement of traffic and tackling congestion

Legal Context

Hampshire County Council was granted powers by the Secretary of State to enforce moving traffic restrictions as part of the Civil Enforcement of Moving Traffic Contraventions Designations and Miscellaneous Amendments Order 2022 on 14 July 2022. The moving traffic enforcement powers will assist the County Council to meet its network management duty by enabling improved enforcement with consequential benefits to road safety, congestion, quality of place and air quality.

Prior to the County Council securing the new powers, the police were the only authority with the jurisdiction to enforce moving traffic restrictions (other than bus lanes and bus gates). Whilst the changes in legislation now enable the County Council to expand its civil enforcement functions, they do not remove the police powers to enforce restrictions as criminal offences. In addition, the police retain sole enforcement for the most severe traffic violations including speeding, traffic signal violations and overtaking on double white lines.

All highway restrictions that are considered for civil enforcement would be subject to a legal Traffic Order. Only established traffic restrictions that fall within scope of the new legislation, and are subject to regular contraventions, would be considered for civil enforcement.

Moving Traffic Enforcement Approach

The County Council aims to take a fair but robust approach to moving traffic enforcement. The aim is to encourage compliance to make roads safer, reduce congestion and to assist with the safe and efficient movement of buses.

The County Council will only consider the civil enforcement of traffic restrictions where actions to improve compliance falls within one or more of the following principles:

Principle 1 Improving Road Safety

Enforcement of moving traffic restrictions at locations where there have been a history of collisions resulting in road users being injured or where there is risk to pedestrians within bus priority measures.

Principle 2 Managing the transport network to ensure the expeditious movement of traffic

Enforcement of moving traffic restrictions at locations where congestion of the network regularly occurs or where contraventions within bus priority measures hinders the efficient movement of buses.

Principle 3 Improving the quality and accessibility of public transport

Enforcement of bus priority measures to assist with the safe and efficient movement of buses, raising the profile and attractiveness of public transport.

Principle 4 Improving the local environment and assisting with carbon reduction

Enforcement of restrictions aimed at easing congestion and reducing delays to emergency services.

Enforcement of restrictions that help improve air quality and encourage more cycling and walking.

Before camera enforcement schemes are considered, the County Council will arrange surveys to establish the level of contraventions. Camera enforcement is costly to implement and maintain and is therefore considered to be a last resort, once all other reasonable steps have been taken to improve compliance of the restrictions by other means.

Traffic signs and road markings associated with highway restrictions will be reviewed and enhanced to make sure they are clear and visible. Vegetation will be cut back, and site lines kept clear so that road users can clearly see the restriction in force as they approach it. Camera enforcement schemes will only be taken forward at locations where road users continue to fail to comply with the restrictions, despite these enhancements.

Financial Context

It is intended that enforcement of moving traffic restrictions will be cost neutral over time, with the full annual costs of operating and maintaining individual enforcement locations met from Penalty Charge Notices (PCN).

As with income from civil on-street parking and bus lane enforcement, any surplus revenue generated through fines from moving traffic enforcement, should it arise, must be spent in accordance with Section 55 of the Road Traffic Regulation Act 1984. Any surplus revenue, once all associated operational and set up costs have been covered, should be prioritised towards other enforcement schemes, measures to improve public transport or other associated highway improvements.

The penalty charge for bus priority contraventions will be in accordance with legislation, currently set at £70, reducing to £35 for motorists who pay within 21 days of the date of issue.

Communications and Engagement

Before the implementation of moving traffic enforcement schemes, the County Council will arrange a six week on-line public consultation accessible on the County Council's website and in line with the Council's consultation principles. The consultation will provide details of the site that has been identified as justifying enforcement and provide the opportunity for comments and suggestions. The consultation will be publicised through various press and media channels.

Prior to the commissioning of enforcement, warning signs to advise motorists of when enforcement will begin will be deployed by the County Council.

During the first six months following implementation of moving traffic enforcement in practice, the County Council will issue warning notices for first-time moving traffic contraventions. The warning notice will set out the six-month

period and advise that any further moving traffic contravention at the same camera location would result in the issuing of a PCN.

Code of Practice

The Policy needs to be read in conjunction with the County Council's Code of Practice (Code of Practice for the Operation Moving Traffic Restrictions), which sets out the objectives and the rules by which the system will be operated. This will ensure privacy, fairness and integrity issues are managed in accordance with relevant guidance and legislation.

Delivery and Operation

Enforcement will be carried out using specialist approved traffic enforcement cameras that utilise Automatic Number Plate Recognition (ANPR) technology and installed in accordance with the manufacturer's specifications.

The County Council will manage the detailed design, procurement, supply, installation, and commissioning of ANPR enforcement cameras, associated signage and road markings.

The enforcement operations (back-office processes including reviewing of captured footage and informal appeals) will be undertaken by the County Council's parking service contractor, with this work being overseen by the County Council's internal Parking Services team. Formal appeals related to bus lane fines will be dealt with by the County Council.

Upon commissioning, ANPR enforcement cameras will detect the number plate of all vehicles entering the bus priority measure and compare that plate to a database of authorised vehicles, known as the 'White-List'. Details of any vehicle that is flagged as being unauthorised will be reviewed and if appropriate, a warning notice or PCN will be issued. The County Council's Parking Services team will maintain overall responsibility for the operational aspects of enforcement, processing arrangements and formal appeals.

UK General Data Protection (UK GDPR)

The County Council and its parking service contractor will process personal data in accordance with data protection legislation, including the UK General Data Protection Regulation (UK GDPR) and the Data Protection Act



**Moving Traffic and Bus Gate Enforcement -
Guidance notes for the enforcement and cancellation of Penalty Charge Notices**

October 2022
Version 1.0

Parking Services
Hampshire County Council
Queen Elizabeth Court
The Castle
Winchester SO23 8UD

parking.services@hants.gov.uk

Policies set out in this document are providing guidance only.
Each case will be considered on its own merits, taking into account all of the evidence
and the exceptionality of the circumstances.

1. Introduction

- 1.1 Hampshire County Council as the Highway Authority has now been granted powers under part 6 of the Traffic Management Act 2004 to enforce moving traffic contraventions on the local highway network.
- 1.2 This Guidance:
 - a. Outlines how moving traffic and bus gate restrictions are enforced by Civil Enforcement Officers (CEO's) in the districts of Hampshire, the process involved for the issue of a Penalty Charge Notice (PCN) and defines each restriction.
 - b. Outlines the process involved for staff to handle each appeal against a penalty charge.
 - c. Clarifies the enforcement and decision-making process, whilst ensuring consistency and transparency in parking operations.
- 1.3 Careful regard has been paid to the Statutory Guidance "The Secretary of State's Statutory Guidance to Local Authorities on the Civil Enforcement of Parking Contraventions" (as stipulated by section 87 of the Traffic Management Act 2004) in the formation of this document.

2. Hampshire's Objectives

- 2.1 The Moving Traffic and Bus Gate Policy and Operation Guidance should be read in context with the Hampshire Local Transport Plan (LTP) 2011-31
- 2.2 <http://documents.hants.gov.uk/transport/HampshireLTPPartALongTermStrategy2011-2031RevisedApril2013.pdf>
- 2.3 The Local Transport Plan sets out the wider transport strategy for the County. Successful management of traffic enforcement can have a major impact on the transport network including:
 - a. Improved road safety
 - b. Reduced congestion
 - c. Improved air quality
 - d. Maintaining access to and encouraging use of public transport
- 2.4 This will ensure HCC can manage the highway network to safeguard the expeditious movement of traffic, (including pedestrians and cyclists), as required under the Traffic Management Act 2004 Network Management Duty 4.

Financial Objectives

- 2.5 The County Council's aim is to operate the moving traffic and bus gate enforcement on a full cost recovery basis, but should the authority make a surplus on its on-street parking account, it will be used in accordance with the legislative restrictions in Section 55 (as amended) of the Road Traffic Regulation Act 1984.
- 2.6 The primary purpose of penalty charges is to encourage compliance with traffic restrictions. In pursuit of this, Hampshire will adopt a charge level consistent with a high level of public acceptability and compliance. All charge levels will be published.

3. Reporting

- 3.1 The transparency given by regular and consistent reporting is important to the acceptance of moving traffic and bus gate enforcement and the wider parking service. Monitoring also provides the authority with management information for performance evaluation and helps to identify where improvements are needed. It also provides a framework for performance comparisons between councils.
- 3.2 Each enforcement authority within Hampshire, excluding the unitary authorities of Southampton and Portsmouth, will produce and publish an annual parking report covering both their on-street and off-street services within six months of the end of each financial year. Annual returns are also required by the Government setting out details relating to the issuing and payment of PCN's and by the Traffic Penalty Tribunal on the number of PCN's issued.
- 3.3 The reports will be produced for publication within the public domain and submission to the Government, the report should include, but is not limited to:

Overview:

Basic information including

- Brief description of car parks,
- On street controls,
- Moving traffic controls,
- Bus gate controls,
- Methods of payments,
- Permit arrangements and
- Numbers of spaces by type (Pay and Display, residents, business, disabled etc.)

3.4 Related issues (Blue Badge information, environmental issues), and Enforcement Overview.

3.5 Changes that have taken place in the year, including new Controlled Parking Zones (CPZ's), tariffs, facilities, consultations, innovations and policy changes.

Financial:

- Total income and expenditure on the on-street and off-street parking and moving traffic accounts.
- Breakdown of income by source (i.e., parking charges, permit income and penalty charges).
- Total surplus or deficit on the on-street parking and moving traffic account.
- Action taken with respect to a surplus or deficit on the on-street account and
- Details of how any financial surplus has been or is to be spent, including the benefits that can be expected as a result of such expenditure.

Statistical:

- Number of higher level PCN's issued.
- Number of lower level PCN's issued.
- Number of PCN's paid (include a percentage of total).
- Number of PCN's paid within 14 days (discount rate) (include a percentage of total).
- Number of PCN's against which a challenge or representation has been made (include a percentage of total).
- Number of PCN's cancelled (i.e., where a challenge or representation is successful) (include a percentage of total).
- Number of PCN's written off for other reasons (include a percentage of total).
- Number of vehicles relocated.
- Number of appeals to adjudicators.
- Number of appeals refused and
- Number of appeals non-contested.

4. Training and Professionalism

- 4.1 All staff working with the Parking Service are integral to its success.
- 4.2 As the front-line staff Civil Enforcement Officers (CEO's) will complete the City and Guilds Level 2 Award for Civil Enforcement Officers (Parking) QCF (No. 1889-01) Course, or the equivalent BPA-WAMITAB qualification within 12 months of being employed. Any staff employed on the Parking Services Contract prior to completing the relevant qualifications must have, as a minimum, core on-street training in parking legislation/codes of practice, contravention codes, conflict resolution and Hampshire County Council's policies regarding equality and diversity, customer service and parking policies and have at least two weeks of shadowing a more experienced CEO to ensure beat familiarisation.
- 4.3 Staff involved in PCN processing must possess a City and Guilds Level 3 award for Notice Processing QCF (No. 1916) or the later BPA-WAMITAB or an equivalent qualification. Staff employed in PCN processing should have completed and attained these formal qualifications within 12 months of being employed.
- 4.4 As an organisation Hampshire County Council and its partners will:
- Treat you as an individual and with respect.
 - Listen to what you say.
 - Be polite, fair, helpful and easy to talk to.
 - Keep what you say to us confidential in line with the Data Protection Act.
 - Continually improve our service to you.
 - Consider your views before we make any changes and be honest about what we can't do.
 - In return, we would ask you to:
 - Be polite and considerate to our staff and
 - Give us relevant information when we ask you for it, including any suggestions for improvement.
 - Our Standards
 - Our aim is to answer queries or complaints at first point of contact or, failing that, to transfer or direct you to someone best placed to assist you and keep you informed along the way.
- 4.5 Letters, faxes and email
- Our aim is to provide an answer within 10 working days. For more complex enquires, our aim is to give you an update within 10 working days and an answer within 20 working days.
- 4.6 Phone
- Our aim is to answer the phone within 20 seconds.

5. Contravention Codes and Observation Times for Moving Traffic and Bus Gate Contraventions

5.1 There is a single, nationwide list of contraventions and associated codes although not all contravention codes apply in all council areas.

Code	Observation Period (mins)	Description
31	0	Entering and stopping in a box junction where prohibited
32	0	Failing to proceed in the direction shown by the arrow on a blue sign
33	0	Using a route restricted to certain vehicles
34	0	Being in Bus Lane
37	0	Failing to give way to oncoming vehicles
38	0	Failing to comply with a sign indicating that vehicular traffic must pass to the specified side of the sign
50	0	Performing a prohibited turn
51	0	Failing to comply with a no entry restriction
52	0	Failing to comply with a prohibition on certain types of vehicle
53	0	Failing to comply with a restriction on vehicles entering a pedestrian zone
54	0	Failing to comply with a restriction on vehicles entering and waiting in a pedestrian zone

6. Guidance on General Grounds for Appeal/Mitigating Circumstances

- 6.1 This section includes guidance regarding the general grounds for appeal which may be submitted against a PCN for any moving traffic or bus gate contravention. The guidance will outline how appeals will be considered and the various factors that will be taken into consideration.
- 6.2 The criterion outlined is for guidance only as each case must be considered on its own merits, taking all circumstances into account.
- 6.3 There is no legal obligation to take account of these mitigating circumstances but in order to act fairly and proportionately the following mitigating circumstance may be taken into consideration.

Emergencies

Appeal Scenario	Action	Notes
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Medical Emergency	Cancel	Where documentary evidence is supplied that a genuine medical emergency caused the contravention.
	Uphold	Where no documentary evidence to support the claim that a medical emergency had occurred or where there was a reasonable alternative available to the driver. <i>Or</i> Where other evidence contradicts the appellants claim.
Emergency Vehicles	Cancel	Where documentary evidence is supplied that the vehicle is an emergency services vehicle (where unmarked) responding to or dealing with an emergency. This may be done by way of a letter or email from the service.
	Uphold	Where no documentary evidence to support the claim that the vehicle is an emergency services vehicle responding to an emergency.
Utility Emergency (Gas, Water, Electrical)	Cancel	Where documentary evidence is supplied that the vehicle was essential for the immediate repair of a gas or water leak, or electrical emergency, and that there was no reasonable alternative for the driver. An emergency is deemed as such where it is an unforeseen event, that poses a risk to life or property and must not be a pre-booked appointment , and as soon as the fault is rectified the vehicle should be moved to a legal alternative, even if additional work is still required at the call out.
	Uphold	Where there is no documentary evidence and the call is not deemed to be an emergency, or the vehicle remained in place for longer than required to rectify the emergency.

Other emergencies	Cancel	Where documentary evidence is supplied that an unforeseen event or circumstance arose, causing the contravention. An emergency is deemed as such where it is an unforeseen event that poses a serious risk to life, health or property.
	Uphold	Where there is no documentary evidence provided, or alternative route was available, the event is not deemed to be an emergency.

General Mitigating Circumstances

Appeal Scenario	Action	Notes
The driver does not feel the PCN was fair, or / and offers a compelling reason why the PCN should be cancelled.	Cancel	The council reserves the right in accordance with operational guidance provided by the statutory body to cancel a PCN where it is felt there are sufficient grounds to do so. In such circumstances the public interest, the community impact, impact on the driver and the perception of Civil Parking Enforcement will be taken into account.
	Uphold	After consideration, the grounds are not considered to be compelling to warrant the cancellation of the PCN.
The driver has significant mental health problems	Cancel	Where documentary evidence is provided to support such a claim.

which caused the contravention.		Where a previously diagnosed significant mental health issue, such as Alzheimer's or Dementia, caused the driver to be forgetful or complacent leading to the vehicle contravening the restrictions.
	Uphold	Where there is no documentary evidence. Where the driver has not got significant mental health issues, or has not been previously diagnosed, or the condition did not directly cause the vehicle to contravene the restrictions. For example, stress or depression would not normally be considered to lead to the driver parking incorrectly.
The driver has been recently bereaved.	Cancel	Where the bereavement is very recent and caused or contributed to the contravention. On the first occasion only.
	Uphold	Where the bereavement is not considered very recent.
The driver was not at the location on the day.	Cancel	If the VRM or make of the vehicle on the PCN are not the same as seen in the images recorded against the case. <i>OR</i> The above details seen in the case images are all correct, but the DVLA returns details that do not match these vehicle details. <i>OR</i> The motorist is claiming their vehicle is cloned and can supply evidence to support this.
The driver was not the registered keeper of the vehicle at the time.	Cancel	If the motorist can supply evidence to support their claim. Re-issue Notice to Owner to new registered keeper.

The registered keeper was not the driver.	Uphold	The registered keeper/owner of the vehicle is responsible for a PCN even if they were not driving at the time of contravention.
Already paid the PCN.	Cancel	If a payment has been logged against the case and the payment clears the remaining balance on the case. <i>OR</i> A payment has not been logged against the case, but the motorist can provide evidence to support their claim that they had already paid for the PCN.
	Uphold	Only a partial payment has been made <i>OR</i> A payment has been made at the discounted rate after the expiry of the discount period.
Motorist wishes to be granted more time to pay the PCN or would like a payment instalment.	Uphold	Penalty charge notices must be paid in full within the timeframe. In some circumstances the discounted amount may be re-instated as a Council goodwill.

Road Markings, signs, or the Issue of the PCN

Appeal Scenario	Action	Notes
Other vehicles were also committing a contravention in this area and did not receive a PCN.	Uphold	The rules are applied to everyone
The driver did not understand the signs or road markings	Uphold	All drivers are expected to understand what each road sign or marking means. Further details on this can be found in the Highway code.
	Cancel	If restriction or tariff boards are incorrect or missing.

The PCN was issued incorrectly; or the correct procedure has not been followed.	Cancel	Where a claim that the correct procedure in the issue of the PCN was not followed is proved, and the not following of that procedure would be a procedural impropriety.
	Uphold	Where a claim that the correct procedure in the issue of the PCN was not followed is not proved, or it would not be a procedural impropriety.
The signs or road markings were worn/missing/incorrect and could not be understood by the driver.	Cancel	Camera footage taken at the time will be checked, and a further site visit of the location will be carried out if necessary. Where the road markings or signage is considered to not inform or confuse the motorist of the restriction.
	Uphold	Camera footage taken at the time will be checked, and a further site visit of the location will be carried out if necessary. The road markings or signs are not required to be in perfect condition at all times, and there is no expectation the council will re paint them at regular intervals, or on every occasion that there are road works. The road markings and signage should be of sufficient quality for the driver to understand that a restriction applies and what that restriction is. Where the road markings or signage is considered to be clearly understandable to a motorist and to conform the legal requirements.
Where the signs or roads markings were covered by leaves, snow, foliage or flooding.	Cancel	Where it can be established that the conditions presented at the time of contravention did not inform the driver of the restrictions.
	Uphold	Where it can be established that the conditions presented at the time of contravention did inform the driver of the restrictions.

		<p>The camera footage will be examined.</p> <p>Where a reasonable alternative indication to the restriction is present for the driver.</p>
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Vehicles carrying out a specific task

Appeal Scenario	Action	Notes
The vehicle is a taxi or bus.	Uphold	There is no general exemption for a bus, private hire or Hackney carriage taxi.
The vehicle was being used in the construction/ demolition of a building.	Cancel	Where the vehicle is actively engaged in the construction or demolition of a building at the time the PCN is issued, and the TRO allows for that type of works.
	Uphold	Where the vehicle is not actively engaged in the building works at the time the PCN is issued.
The vehicle was part of a wedding or funeral cortege.	Cancel	Only where the vehicle is directly part of the cortege for the ceremony.
The vehicle was being used for cash in transit, or to drop or collect large sums of money from a property.	Cancel	Where documentary evidence is supplied that the vehicle is a high security vehicle (bullion), actively engaged in the collection or dropping off of cash amounts to a commercial premises.
	Uphold	<p>Where there is no documentary evidence provided, or the driver was deemed not to be engaged in the collection/delivery when the PCN was issued.</p> <p>Also, where the vehicle is not a high security (bullion) vehicle.</p>

7. PCN Issue and appeal guidelines for specific contravention codes/Statutory Appeals

- 7.1 This section includes guidance on general grounds for appeal against a specified contravention and how it may be established whether a statutory ground for appeal has been satisfied.
- 7.2 Listed in this section are appeals that may be submitted against a PCN for a specific contravention (This is where an appeal scenario may be valid against one contravention but not all) and corresponding guidance on how that appeal may be assessed and to the acceptance or rejection of that appeal scenario.
- 7.3 In all cases, notes and photographs will be taken by the CEO and the appeals team will review these to check for any conflicts of information. The criteria are for guidance only as each case must be considered on its own merits, taking into account all circumstances.
- 7.4 A list of moving traffic and bus gate contraventions can be found under section 5.1 of this document.

Code 31,32 &33:

- 7.5 Guidance will be issued by the County Council should these contraventions be introduced in the future.

Code 34: Being in a Bus Lane

Appeal Scenario	Action	Notes
I was preventing an accident from happening	Cancel	If sufficient evidence is provided which corresponds with CCTV footage
I was directed by the Police or Civil Enforcement Officer to use the bus gate	Cancel	If sufficient evidence is provided which corresponds with CCTV footage
I have a disabled persons blue badge	Uphold	Blue Badge holders are not exempt
I am a taxi driver/delivery driver	Uphold	Taxi drivers and delivery drivers are not exempt

Code 37, 38, 50, 51, 52, 53 &54:

7.6 Guidance will be issued by the County Council should these contraventions be introduced in the future.

HAMPSHIRE COUNTY COUNCIL

Decision Report

Decision Maker:	Leader and Executive Member for Hampshire 2050 and Corporate Services
Date:	9 March 2023
Title:	Andover Road (Winchester) Strategy
Report From:	Assistant Chief Executive and Director of Hampshire 2050

Contact name: David Jowsey

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Email: David.Jowsey@hants.gov.uk

Purpose of this Report

1. The purpose of this report is to agree the Andover Road Corridor Strategy.
2. Changes have taken place which have a bearing on the Andover Road Corridor and mean it is timely for the County Council to take a view on what strategy and key measures should and can apply to the corridor. These include the development and agreement of:
 - a. The adopted Winchester Movement Strategy which sets a city-wide framework for change including a need to reduce traffic levels in the City Centre and inclusion of a site for a northern park and ride;
 - b. The draft Winchester City Council Local Plan Reg 18 and indicative suggestion that sites to the North of Barton Farm may now feature in the next local plan;
 - c. Improvements to M3 Junction 9, which stand to change some strategic routing options for traffic, potentially impacting Andover Road.
3. A report is needed now in order that the County Council is well placed to input into the evolving local plan, to National Highways proposals for Junction 9 of the M3, and also to respond to public representations that have been made to change the plan to close Andover Road to all traffic.

Recommendations

4. That the Leader and Executive Member for Hampshire 2050 and Corporate Services notes the current representation made by Kings Barton Residents Association to keep Andover Road open to through traffic.
5. That the Leader and Executive Member for Hampshire 2050 and Corporate Services notes that the County Council as Highway Authority is not lawfully empowered to meet this request and that any change to the planning

permission can only be achieved through a full planning application from the developer to the Local Planning Authority.

6. That the Leader and Executive Member for Hampshire 2050 and Corporate Services agrees the elements of the transport strategy proposed in this report and delegates authority to the Director of Hampshire 2050 to safeguard land as necessary, and to develop, design and engage on the transport strategy and its component schemes.

Executive Summary

7. The Winchester Movement Strategy (WMS) was adopted in 2019 by the County Council and Winchester City Council following an extensive process of public engagement. The WMS identified the Andover Road corridor as playing an important role in reducing city centre traffic levels and that it may need to include a new Park and Ride site because other sites in Winchester serve demand from the South. Technical work has since identified that there is a good case for a northern Park and Ride site and that supporting bus priority measures would make it more attractive for users.
8. The emerging local plan for Winchester has identified future development sites on the northern stretch of Andover Road. These have the potential to facilitate a northern Park and Ride being delivered and change the travel and traffic demand on the corridor.
9. National Highways, which operates the Strategic Road Network (motorway), has articulated what changes it plans to make to Junction 9 of the M3. It includes significant alteration to the A33/A34, which in turn stands to change the way traffic accesses the city from the North and East. It has an impact on the Andover Road Corridor as well as other routes into the city. The nature of changes to the Cart and Horses junction stands to have a significant impact on how traffic redistributes following the changes and needs careful consideration so that the changes complement the objectives of the WMS.
10. Planning permission for the Kings Barton development was granted in 2012 by the Secretary of State (SoS) following an Appeal. The permission includes 2,000 homes, district centre, primary school and a 200 space park and ride "light". The planning permission included the re-routing of through traffic that currently uses Andover Road through the development site (along the Winchester Avenue spine road) and the closure of Andover Road to traffic.
11. A petition by Kings Barton Resident Association, with 3,000 signatures, and lobbying by some local resident groups and individuals has called for Andover Road to be kept open to through traffic. The County Council response to the petition stated that as Highway Authority it was not the determining Planning Authority on this matter and that the County Council cannot lawfully unilaterally undo this planning application. This remains the case.
12. This paper considers the issues and opportunities above and seeks to approve a strategy for Andover Road and the north of Winchester which:
 - promotes the introduction of a large Park and Ride site to the north of Winchester accessed off Andover Road;

- supports the potential use of Andover Road, once closed to through traffic, as a bus priority corridor when and if a large northern Park and Ride site is completed;
 - provides bus priority on Andover Road in the vicinity of the Worthy Lane junction (a scheme likely to take the form of a new signal junction with bus priority facilities and potentially requiring a land safeguarding as part of the emerging local plan process);
 - improves the Cart and Horses junction in light of the M3 Junction 9 proposed improvements (also potentially requiring a land safeguarding as part of the emerging local plan process);
 - provides improvements for people walking and cycling between Kings Barton and the city centre; in the short term via Abbots Barton and in the longer term via a direct route along Andover Road; and
 - monitors and manages the impacts of traffic on surrounding roads following the diversion of Andover Road.
13. Introducing a bus priority corridor on Andover Road would be subject to engagement, technical assessment and a business case.
14. This approach is considered to be aligned with the planned new Hampshire Local Transport Plan 4, the Winchester Movement Strategy and responds to the emerging Winchester City Council Local Plan.

Development Context and Local Plan

15. The Kings Barton development was given planning permission in 2012 by the then Secretary of State for the Department of Communities and Local Government following initial refusal by Winchester City Council, the Local Planning Authority, and subsequent appeal by the developer CALA Homes.
16. As part of the appeal, a Statement of Common Ground on Highways and Transport Issues was published in January 2011. Hampshire County Council accepted the traffic volume assessment which underpinned the proposal to realign Andover Road through the development and closing Andover Road to through vehicular traffic. It is important to note that a statement of Common Ground is about what is technically agreeable not what is in dispute. Hampshire County Council's agreement was that the diversion would have sufficient capacity.
17. The developer is obliged to deliver the new development spine road, termed Winchester Avenue, and associated junction changes prior to the occupation of 650 housing units in the development. Based on the developer's estimates the highway changes are likely to be delivered around mid-2025.
18. There has been public concern expressed over the closure of Andover Road to through traffic and this has culminated in a petition which was submitted in July 2022. The petition had over 3,000 signatories and asked Hampshire County Council and CALA Homes to reconsider the current planning permission and instead keep Andover Road open for vehicular traffic. The concerns raised focussed on the impacts of the road diversion on the new

residents of Kings Barton including noise, air quality, severance and safety implications from traffic passing through the development.

19. The County Council responded to the petition stating that it was the Highway Authority and not the determining Planning Authority on this matter. In such circumstances the County Council cannot lawfully and unilaterally undo this planning decision. This means the County Council has no power to direct the developer to keep Andover Road open to all traffic.
20. As the County Council has done through the development of the WMS, it will be appropriate for the County Council to continue to engage with all parties including the developer and Local Planning Authority to satisfy themselves that the closure of Andover Road to vehicular traffic as set out in the planning application is necessary. The County Council will continue to work with the developer and its designer to ensure new highway within the development is fit for purpose as far as possible within the constraints of the approved planning position.
21. In October 2022 Winchester City Council commenced consultation of its draft Regulation 18 Local Plan which sets out how development will be planned and delivered across the whole of Winchester District up to 2039. The draft Local Plan includes a number of proposed sites relevant to this report:
 - Sir John Moore Barracks: mixed use site comprising 750-1,000 homes with a Park and Ride facility provided with a capacity of approximately 850 spaces;
 - Station Approach regeneration area: mixed use development incorporating sites around the station including the Cattle Market car park; and
 - site adjacent to Cart and Horses Junction which will require highway changes to access and could help contribute towards funding for the future improvement scheme.

Andover Road and Current Movement Strategy Context

22. The Andover Road Corridor as defined in this report runs from the Three Maids Hill roundabout at the junction with the A34 to the Carfax junction with City Road. At the northern end it has a National Speed Limit before reducing to 50 mph near the junction to Sir John Moore Barracks. There is a short section of dual carriageway up to the new northern access to the Kings Barton development at Manley Road. The speed limit reduces to 40mph near the new signalised crossing of Andover Road. Andover Road becomes more urban in nature at this point and then the speed reduces to 30mph after the new southern access to Kings Barton.
23. According to permanent traffic counter average annual daily traffic flows are approximately 11,000 vehicles at the northern end and 10,000 vehicles at the southern end. According to surveys in autumn 2022; the middle section which would be subject to the closure to through traffic has daily traffic flows of 7,500 vehicles. Overall traffic levels did not change from 2003 to 2019 and are

currently around 5% below 2019 levels. Andover Road experiences some congestion at peak hours on the approach to the Carfax junction. There is a poor level of service for active travel users along the length of Andover Road and at present a limited public transport service; though a new bus service is planned to be introduced to serve the Kings Barton development.

24. The Winchester Movement Strategy includes a proposal for a northern park and ride site. It also references the potential for Andover Road to include new bus priority measures for park and ride bus services. The rationale for these proposals was to support a reduction in city centre traffic by enabling modal shift in journeys to the city centre from the north.
25. The other key route into Winchester from the north is the B3047 London Road/ Worthy Road corridor and the Cart and Horses junction is an important access point both to Winchester and the strategic road network via the A33 and A34.
26. The County Council has previously stated it would re-examine the case for changes to the Cart and Horses junction in light of proposals promoted by National Highways related to M3 Junction 9 improvements. Engagement has been on-going between the County Council and National Highways in to how the Cart and Horses junction may be altered. The County Council is seeking junction changes that would improve facilities for people walking and cycling and support the objectives of the Winchester Movement Strategy specifically reducing city centre traffic by managing the attractiveness of Worthy Road for through traffic.

North Winchester Transport Strategy – Technical Review

27. In light of the Winchester Movement Strategy objectives and proposal, the emerging local plan but also public representation to keep Andover Road open, a technical study was commissioned by the County Council to inform the corridor strategy and plan. It was undertaken by WSP transport planning consultants.
28. The consultants looked at Andover Road in three sections. The first section was from the Three Maids Hill roundabout to the Harestock Road junction, the second was between the Harestock Road and Stoney Lane junctions (in the area of Kings Barton). The third was from the Stoney Lane area to the Carfax Junction. The technical review looked at current traffic flows and the capacities of various corridor options.
29. The conclusions were that the corridor strategy most likely to be deliverable and achieve the objectives of the movement strategy would involve a plan that consists of:
 - a new large Northern Park and Ride site off Andover Road;
 - enhanced pedestrian and cycle links from Kings Barton and Kings Worthy to central Winchester; and
 - delivery of bus priority measures on the Andover Road corridor including at the middle and southern sections of Andover Road.

30. Between the Harestock Road and Park Road junctions, the plan would involve:
- in line with the agreed planning permission the re-routing of through traffic movements that currently use Andover Road through the Kings Barton site;
 - the closure of Andover Road to vehicular traffic except buses in the vicinity of Henry Beaufort School;
 - the creation of a pedestrian and cycle route along the route of Andover Road; and
 - provision of local vehicular access to residential properties on Andover Road via Kings Barton.
31. Whilst the concerns about noise, air quality, severance and safety implications from traffic passing through the Barton Farm development were given due regard the fact remains that the request to undo the planning permission reached by the SoS is not in the Highway Authority's gift to change. The technical work has shown the level of traffic flow anticipated to pass through the development would be around 10,000 vehicles. Many Hampshire streets of a similar type carry similar levels of traffic, so it is not unusual. However, in a placemaking context such traffic flows are not compatible with the ambitious vision originally set out by the developer for a "shared space" (where there is little or no delineation or level difference between areas for vehicles and people) environment.
32. Hampshire County Council is discussing detailed design issues with the developer and its designer, to ensure that through the Section 278 design approval process the highway will be fit for purpose as far as reasonably practicable within the constraints of the approved planning position. Safety concerns which have been raised will need to be addressed by the developer and its designer involving defined level differences and measures like kerbs and informal crossing facilities.

National Highways Junction 9 and Cart and Horses Junction

33. The Cart and Horses junction in Kings Worthy is a staggered priority junction located on the A33 where it intersects the B3047. In light of the proposed highway changes by National Highways at M3 Junction 9, and a strong local desire to see the junction redesigned to better accommodate vehicular movements the County Council will develop proposals to improve the junction.
34. At the Cart & Horses junction initial optioneering design work has been undertaken to examine different junction options in light of the National Highways proposed changes to the A30 as part of the M3 Junction 9 project. The design options are for either a signalised junction, or two compact roundabouts. No funding has been identified to deliver a scheme, so the County Council will work with National Highways to develop a delivery and funding plan.

35. Once designed it may be necessary to safeguard the preferred scheme design for the junction improvement. This will need to be done through the local plan process. The funding of the scheme may require borrowing in advance of development taking place which would in due course be paid back from local development.

Finance

36. Further work will be required to assess the case for bus priority on Andover Road once the location and size of the northern Park and Ride is confirmed, this will include design considerations for how bus priority connects with the highway network. Any further study work will be funded through existing revenue resources.
37. In terms of the finance elements of making improvements to the Cart and Horses junction, feasibility and preliminary design work, and associated activities will be funded through existing revenue resources. This is likely to require a call upon local and national funding sources which have not yet been secured. This may need to include a mix of Local Transport Plan Grant funding, locally held Community Infrastructure Levy funding (subject to the agreement of Winchester City Council), borrowing against future development contributions (potentially available if the Local Plan allocates certain sites) and national funding made available via National Highways. At this time there is no certainty that a funding plan can be achieved but best endeavours will be made to do so.

Consultation and Equalities

38. The Winchester Movement Strategy has had considerable engagement as part of its adoption and subsequent prioritised action plan.
39. As part of the most recent engagement exercise on the action plan undertaken between December 2021 to February 2022, views were sought on the proposals for a northern Park and Ride and associated bus priority measures along Andover Road; including the use of the Andover Road as a bus priority corridor.
40. The engagement had 896 responses and there was a high level of agreement that a new northern P&R and associated interventions would support the priorities of Winchester Movement Strategy; 72% agreed it would reduce city centre traffic and 64% agreed it would support sustainable growth. A total of 44 respondents provided comments that they disagreed with the closure of Andover Road to through traffic.
41. Full results of the 2022 engagement can be found on the Council website: <https://documents.hants.gov.uk/transport/WMS-NextStepsProposals-LCWIP-EngagementFindingsReport.pdf>
42. The Andover Road strategy outlined in this report has been discussed with relevant Local County Councillors and Winchester City Council. Both the local councillors and Winchester City Council were supportive of the general

approach, but it is recognised there are detailed considerations that need to be resolved.

43. This specific decision is considered to have a neutral impact on people with protected characteristics as this report is associated with a policy direction. As proposals outlined within this report are developed further engagement and consideration of the equality impacts will be required.

Climate Change Impact Assessments

44. Hampshire County Council utilises two decision-making tools to assess the carbon emissions and resilience of its projects and decisions. These tools provide a clear, robust, and transparent way of assessing how projects, policies and initiatives contribute towards the County Council's climate change targets of being carbon neutral and resilient to the impacts of a 2°C temperature rise by 2050. This process ensures that climate change considerations are built into everything the Authority does.

Climate Change Adaptation

45. Approval of the Andover Road Strategy will have no impact on climate change itself, however climate change adaptation is a key consideration for the Local Planning Authority and Local Highway Authority in assessing any future development proposals.

Carbon Mitigation

46. Approval of the Andover Road Strategy will have no impact on climate change itself, however climate change mitigation is a key consideration for the Local Planning Authority and Local Highway Authority in assessing any future development proposals.

REQUIRED CORPORATE AND LEGAL INFORMATION:

Links to the Strategic Plan

Hampshire maintains strong and sustainable economic growth and prosperity:	yes
People in Hampshire live safe, healthy and independent lives:	yes
People in Hampshire enjoy a rich and diverse environment:	no
People in Hampshire enjoy being part of strong, inclusive communities:	no

Other Significant Links

Links to previous Member decisions:	
<u>City of Winchester Movement Strategy</u>	19 Sept 2017
<u>Winchester Movement Strategy</u>	23 April 2019
<u>Winchester Movement Strategy – Consultation and Engagement</u>	18 Nov 2021
Direct links to specific legislation or Government Directives	
<u>Title</u>	<u>Date</u>

Section 100 D - Local Government Act 1972 - background documents	
<p>The following documents discuss facts or matters on which this report, or an important part of it, is based and have been relied upon to a material extent in the preparation of this report. (NB: the list excludes published works and any documents which disclose exempt or confidential information as defined in the Act.)</p>	
<u>Document</u>	<u>Location</u>
None	

EQUALITIES IMPACT ASSESSMENT:

1. Equality Duty

The County Council has a duty under Section 149 of the Equality Act 2010 ('the Act') to have due regard in the exercise of its functions to the need to:

- Eliminate discrimination, harassment and victimisation and any other conduct prohibited by or under the Act with regard to the protected characteristics as set out in section 4 of the Act (age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation);
- Advance equality of opportunity between persons who share a relevant protected characteristic within section 149(7) of the Act (age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation) and those who do not share it;
- Foster good relations between persons who share a relevant protected characteristic within section 149(7) of the Act (see above) and persons who do not share it.

Due regard in this context involves having due regard in particular to:

- The need to remove or minimise disadvantages suffered by persons sharing a relevant characteristic connected to that characteristic;
- Take steps to meet the needs of persons sharing a relevant protected characteristic different from the needs of persons who do not share it;
- Encourage persons sharing a relevant protected characteristic to participate in public life or in any other activity which participation by such persons is disproportionately low.

2. Equalities Impact Assessment:

This specific decision is considered to have a neutral impact on people with protected characteristics as this report is associated with a policy direction. As proposals outlined within this report are developed further engagement and consideration of the equality impacts will be required.

HAMPSHIRE COUNTY COUNCIL

Decision Report

Decision Maker:	Leader and Executive Member for Hampshire 2050 and Corporate Services
Date:	9 March 2023
Title:	Fleet and Surrounding Areas Transport Schemes Update
Report From:	Director of Hampshire 2050

Contact name: Hayley Thorn

Tel: 0370 7793 526

Email: hayley.thorn@hants.gov.uk

Purpose of this Report

1. The purpose of this report is to provide an update on local transport priorities in Fleet and the surrounding areas and gain agreement for a shortlist of potential schemes that should be investigated through commencement of feasibility studies.

Recommendation

2. That the Leader and Executive Member for Hampshire 2050 and Corporate Services approves the revised scheme priorities for Fleet as set out in paragraph 20 of this report.

Executive Summary

3. There is a need for the County Council to amend the scheme priorities in Fleet to reflect a changing funding climate and practical considerations around spending locally held Section 106 developer contributions.
4. In recent years there has been various development in Fleet and the surrounding areas, including sizeable developments at Queen Elizabeth Barracks, Church Crookham, and Hitches Lane, Fleet. To mitigate the impact of such developments on the highway network, developer contributions were taken to fund delivery of local transport improvement schemes. The opportunity to use contributions to match fund bids for Government Funding led to a previous scheme prioritisation that has the Fleet Station Junction scheme as the number one priority. The value of the scheme was in excess of what was held hence the approach to seek to attract additional funding. At the time, the Government's Pinch Point funding looked like a good opportunity and whilst Hampshire submitted a bid the fund was subsequently absorbed into what is now called the Levelling Up Fund. The criteria for bidding was then changed with the focus on priority areas for levelling up. Fleet is not listed as a priority area for levelling up.

5. There is now a need to change the approach to prioritisation to deliver the best package of scheme that can be delivered with the funding held locally and before the contributions expire.

Contextual information

6. In 2011 the Fleet Town Access Plan (TAP) was published which set out the transport strategy for Fleet and the surrounding areas of Church Crookham, Elvetham Heath and Ancells Farm. The Fleet TAP sought to provide a vision for improved accessibility for the area which could be used to inform future development plans and guide applications for transport infrastructure funding. At the time of preparing the Fleet TAP, Local Transport Plan 2 was the County Council's countywide transport strategy, with LTP3 adopted later in 2011, following the publication of the Fleet TAP.
7. The Fleet TAP and LTPs formed the basis of the local transport strategy that was used to inform planning decisions on highway matters and provided a basis for requesting developer contributions to mitigate the impact of development in the area. Much of the funding secured through developer contributions has now been spent on schemes relevant to the local transport strategy, however a significant amount of funding remains. The policy framework has moved on.
8. In 2022 the County Council consulted on the draft LTP4. This new transport strategy proposed a step change in the county council's approach to transport planning, with a shift away from planning for vehicles, towards planning for people and places. The new LTP is guided by two core principles: to significantly reduce dependency on the private car; and to provide a transport system that promotes high quality, prosperous places and puts people first. These guiding principles and associated policies within the LTP4 guide the county council's planning, delivery and operation of transport functions and infrastructure.

Developer Contributions Held

9. In 2010, planning permission was granted for a mixed use development at the Queen Elizabeth Barracks (QEB) in Church Crookham. The development consisted of up to 900 dwellings, 7,500 sqm of office space, a community centre, primary school, convenience store and greenspace. In order to mitigate the transport impact of this development, a Section 106 (S106) agreement was agreed in 2011 which required the developers to make a contribution of £3million to be used for off-site mitigation, including for measures identified in the Fleet TAP.

Previous Allocation of Developer Contributions

10. As part of the S106 agreement for the QEB development, Hart District Council was required to establish a steering group of local representatives to make recommendations to the County Council regarding allocations of the developer contributions. The steering group comprises representatives of the County Council, District Council and Church Crookham Parish Council. The steering group has been meeting since 2015, with the County Council taking due consideration of the steering group's views before committing expenditure of the QEB developer contributions.

11. In 2018, following engagement with the QEB steering group, a decision report was taken to the County Council's Executive Member for Environment and Transport that made recommendations to commence design work on four schemes, using the QEB funding. These schemes were:
 - Fleet Station Roundabout- Highway capacity improvements;
 - Windy Gap Junction- Congestion reduction;
 - Beacon Hill Road Corridor- Cycling improvements; and
 - Elvetham Heath Double Roundabouts- Safety and capacity improvements.
12. The recommendations within the 2018 report were approved and work was progressed to develop the above listed schemes. However, since this time only the Elvetham Heath/Fleet Road and Reading Road North/Hitches Lane double roundabout scheme has progressed to delivery. This scheme is currently being delivered through Section 278 works for the Hitches Lane development.
13. In 2020 a funding bid was submitted to the Department for Transport's (DfT) pinch point fund for the capacity improvements proposed to the Fleet Station Roundabout on the A3013. This funding opportunity was withdrawn by the DfT and a review of the business case for the scheme was undertaken in light of the emerging new emphasis on people and place. In the absence of funding, the review determined the scheme was not deliverable.
14. The case for progressing the congestion reduction scheme at the Windy Gap on the A323 at the junctions of Fleet Road and Aldershot Road, has also been reviewed. Whilst there may be a need for the scheme in the future, post-covid traffic levels are lower than previously predicted, and it is therefore considered that other schemes may be more aligned to the new Local Transport Plan.
15. Initial investigations have been undertaken into the feasibility of providing cycling infrastructure along the Beacon Hill Road corridor. However, since these investigations were undertaken an improved cycling connection has been provided by the shared use path that has been installed along Leipzig Road and Naishes Lane. The need for the Beacon Hill Road scheme has changed and any further study work is refocused on reviewing the provision of cycling infrastructure along this corridor, drawing on the outputs of the Hart Local Cycling and Walking Infrastructure Plan (LCWIP) that is currently in development.

Revised Priorities

16. To inform the revised list of priorities for use of transport contributions in Fleet and surrounding areas, County Councillors representing the Fleet Town and Church Crookham and Ewshot Divisions were asked to share their views on residents' priorities for the area. Feedback was also sought from Hart District and Parish Councillors via the QEB Steering Group. The priorities identified through this engagement exercise are as follows (in no particular order):
 - installation of pedestrian crossings on Reading Road South;
 - improved walking and cycling links between Church Crookham and Calthorpe Park School;

- installation of pedestrian crossing in Crookham Village as part of wider links to Calthorpe Park School;
- review of traffic movements on Malthouse Bridge;
- banning right turns from the supermarket on Beacon Hill Road and the petrol station on Reading Road South;
- installation of a pedestrian crossing on Hitches Lane to enable crossing between the Hart Leisure Centre and Calthorpe Park School;
- improved walking and cycling routes on Hitches Lane between Elvetham Road roundabout and Emerald Avenue roundabout;
- measures to reduce vehicle speeds at the junction of Elvetham Road and Church Road;
- review of the traffic arrangements and pedestrian crossing facilities on the approach to Fleet Station;
- review the safety of the Upper Street junctions;
- Safety Improvements at the junction of Tweseldown Road and Beacon Hill Road Junction; and
- extension of the cycle lane on Norris Hill Road to Windy Gap Junction.

17. Work has previously been undertaken to review the Tweseldown Road and Beacon Hill Road junction, with a design to install a roundabout being considered undeliverable due to site constraints and high costs. However, this does not preclude further assessment being undertaken to consider lower cost interventions at this location. The extension to the Norris Hill cycle lane has also previously been investigated and the County Council is liaising with the developers of Hartland Village for this stretch of cycling infrastructure to be provided under Section 278 works.

18. Before any of the above proposals could be taken forward to scheme development, evidence would need to be gathered on the need for intervention, and the proposals assessed against the priorities within the emerging LTP4 and the Hart LCWIP once it has been developed. Once the strategic case and rationale for each proposal is established feasibility studies can be undertaken. The cost and time to undertake feasibility studies will vary for each of the schemes due to their varying scale and complexity and the capacity of County Council officers to undertake the work.

19. Several of the developer contributions held for the Fleet and surrounding areas have expiry dates attached to them, meaning that work should be undertaken to develop and deliver schemes as soon as possible to reduce the risk of monies having to be returned to the developers. As such it is recommended that work commences on undertaking feasibility studies for the above listed local priority schemes, except for the extension to the Norris Hill cycle way given the ongoing Section 278 discussions for this scheme.

20. It is noted that the developer contributions held for Fleet and the surrounding area is limited and will not be sufficient to fund development and delivery of all the priorities that have been identified. Therefore, a small number of schemes should be progressed for further study, with other schemes on the list revisited at a later date as funding allows. It is recommended that the following schemes are progressed for further study:

- installation of pedestrian crossings on Reading Road South;
- installation of a pedestrian crossing on Hitches Lane to enable crossing between the Hart Leisure Centre and Calthorpe Park School;
- improved walking and cycling routes on Hitches Lane between Elvetham Road roundabout and Emerald Avenue roundabout;
- improved walking and cycling links between Church Crookham and Calthorpe Park School;
- banning right turns from the supermarket on Beacon Hill Road and the petrol station on Reading Road South;
- safety improvements at the Albert Street, Clarence Road and Connaught Road junctions with Upper Street;
- safety improvements at the junction of Tweseldown Road and Beacon Hill Road; and
- review of traffic arrangements and pedestrian crossing facilities on the approach to Fleet Station.

21. The schemes recommended for further study have been subject to initial feasibility studies and further work would be required to develop detailed proposals. These schemes would support residents of Fleet and Church Crookham in accessing local facilities and services using active modes by adding to the network of safe and convenient walking and cycling routes. There is a strong policy basis within the emerging LTP4 for prioritising these schemes as they accord with the two guiding principles as well as a number of the core policies, including:

- putting people and places at the heart of our decisions (policy C1);
- transport strategies and schemes to be developed in accordance with consideration of all users (policy C3);
- support local living and reduce demands on transport (policy C5); and
- encourage sustainable travel behaviours (policy C6).

22. The Fleet TAP is also supportive of progressing these schemes as it recommends installation of pedestrian crossings on Reading Road South (scheme references PC17 and PC18 of the Fleet TAP) improved crossing links to Calthorpe Park School and improved cycling links along the northern length of Hitches Lane (scheme references PC5 and PC6 of the Fleet TAP).

Finance

23. There is £6.4million in developer contributions held for the Fleet and Church Crookham. Some of this funding has already been allocated to specific schemes as specified in the individual S106 agreements.

24. There is currently £2.1million in developer contributions held for the Fleet and Church Crookham area that is not currently allocated to a specific scheme. There is also £3.8million in developer contributions held for delivery of measures identified in the Fleet TAP, a number of which are identified as priorities in this report. It is anticipated that all the funding that is not already allocated for specific uses will be needed to progress the design and delivery of the schemes recommended for prioritisation in paragraph 20. If, however any funding remains, or further developer contributions are secured, feasibility studies will be undertaken for other schemes listed in paragraph 16.
25. Where the terms of the S106 agreements allow, developer contributions will be used to fund the feasibility and design work that is required to develop the prioritised schemes. Where S106 monies cannot be used in this way, feasibility studies will be funded through the County Council's core revenue budget or external funding sources, with delivery of schemes funded through developer contributions.

Consultation and Equalities

26. The proposals within this report have been shaped by engagement with County, District and Parish Councillors as representatives of their local communities. The most recent meeting of the QEB steering group was held on 20 January 2023 where proposals for transport improvements relating to the QEB development were discussed. These discussions were framed within the context of the County Council's emerging transport strategy, LTP4, which encourages a shift away from planning for vehicles, towards planning for people and places.
27. A neutral impact on people with protected characteristics has been identified from this decision. The undertaking of feasibility studies will not result in any change and therefore tangible impact. It is the delivery of schemes at a later stage that will deliver change and therefore will be subject to their own equalities impact assessments. Therefore, whilst any transport schemes that are identified as a result of the feasibility studies are expected to have positive impacts on a range of protected characteristics such as age, disability, pregnancy & maternity, poverty and rurality by providing improved access and connectivity by non-motorised transport, they will be subject to their own Equalities Impact Assessment as schemes progress to delivery.

Climate Change Impact Assessments

28. Hampshire County Council utilises two decision-making tools to assess the carbon emissions and resilience of its projects and decisions. These tools provide a clear, robust, and transparent way of assessing how projects, policies and initiatives contribute towards the County Council's climate change targets of being carbon neutral and resilient to the impacts of a 2°C temperature rise by 2050. This process ensures that climate change considerations are built into everything the Authority does.
29. The development of feasibility studies, as is proposed in this report will not, in itself have any discernible impact on climate. Any impacts will come at a later

stage should any schemes be taken forward for delivery. However, in due course, the delivery of measures to improve walking and cycling facilities in Fleet and the surrounding area has potential to reduce carbon emissions due to a reduction in car trips that could be achieved.

Climate Change Adaptation

30. The climate change adaptation tool was not applicable to this report because the decision relates to a number of possible individual projects, which are subject to assessment individually at the point at which they come forward. The document itself is more strategic in nature and therefore doesn't have a direct impact on climate change.

Carbon Mitigation

31. The climate change mitigation tool was not applicable to this report because the decision relates to a number of possible individual projects, which are subject to assessment individually at the point at which they come forward. The document itself is more strategic in nature and therefore there are no clear mitigation measures which could be taken forward at this time.

REQUIRED CORPORATE AND LEGAL INFORMATION:

Links to the Strategic Plan

Hampshire maintains strong and sustainable economic growth and prosperity:	Yes
People in Hampshire live safe, healthy and independent lives:	Yes
People in Hampshire enjoy a rich and diverse environment:	Yes
People in Hampshire enjoy being part of strong, inclusive communities:	Yes

Other Significant Links

Links to previous Member decisions:	
<u>Title</u> Decision - Church Crookham Transport Strategy About the Council Hampshire County Council (hants.gov.uk)	<u>Date</u> 24/04/2018
Direct links to specific legislation or Government Directives	
<u>Title</u>	<u>Date</u>

Section 100 D - Local Government Act 1972 - background documents	
<p>The following documents discuss facts or matters on which this report, or an important part of it, is based and have been relied upon to a material extent in the preparation of this report. (NB: the list excludes published works and any documents which disclose exempt or confidential information as defined in the Act.)</p>	
<u>Document</u>	<u>Location</u>
None	

EQUALITIES IMPACT ASSESSMENT:

1. Equality Duty

The County Council has a duty under Section 149 of the Equality Act 2010 ('the Act') to have due regard in the exercise of its functions to the need to:

- Eliminate discrimination, harassment and victimisation and any other conduct prohibited by or under the Act with regard to the protected characteristics as set out in section 4 of the Act (age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation);
- Advance equality of opportunity between persons who share a relevant protected characteristic within section 149(7) of the Act (age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation) and those who do not share it;
- Foster good relations between persons who share a relevant protected characteristic within section 149(7) of the Act (see above) and persons who do not share it.

Due regard in this context involves having due regard in particular to:

- The need to remove or minimise disadvantages suffered by persons sharing a relevant characteristic connected to that characteristic;
- Take steps to meet the needs of persons sharing a relevant protected characteristic different from the needs of persons who do not share it;
- Encourage persons sharing a relevant protected characteristic to participate in public life or in any other activity which participation by such persons is disproportionately low.

2. Equalities Impact Assessment:

A neutral impact on people with protected characteristics has been identified from this decision. The undertaking of feasibility studies will not result in any change and therefore tangible impact. It is the delivery of schemes at a later stage that will deliver change and therefore will be subject to their own equalities impact assessments. Therefore, whilst any transport schemes that are identified as a result of the feasibility studies are expected to have positive impacts on a range of protected characteristics such as age, disability, pregnancy & maternity, poverty and rurality by providing improved access and connectivity by non-motorised transport, they will be subject to their own Equalities Impact Assessment as schemes progress to delivery.

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HAMPSHIRE COUNTY COUNCIL

Decision Report

Decision Maker:	Leader and Executive Member for Hampshire 2050 and Corporate Services
Date:	9 March 2023
Title:	Rushmoor Local Cycling and Walking Infrastructure Plan
Report From:	Assistant Chief Executive and Director of Hampshire 2050

Contact name: Hayley Thorn

Tel: 0370 7793 526

Email: hayley.thorn@hants.gov.uk

Purpose of this Report

1. The purpose of this report is to seek approval of the Local Cycling and Walking Infrastructure Plan (LCWIP) for Rushmoor which has been developed in partnership with Rushmoor Borough Council, supported by consultants Sustrans.

Recommendations

2. That the Leader and Executive Member for Hampshire 2050 and Corporate Services approves the Rushmoor Local Cycling and Walking Infrastructure Plan (LCWIP).
3. That the Leader and Executive Member for Hampshire 2050 commends the LCWIP to Rushmoor Borough Council for endorsement and inclusion in its own policy framework.

Executive Summary

4. This paper seeks to outline the work that has been undertaken in preparing the Rushmoor LCWIP and seeks approval of the LCWIP document (which can be found here: <https://documents.hants.gov.uk/transport/Rushmoor-LCWIP-Report.pdf>) as part of the transport strategy for Rushmoor Borough.
5. The Rushmoor LCWIP was developed in partnership between Hampshire County Council and Rushmoor Borough Council, with support from consultants Sustrans, following the latest government guidance. The purpose of the LCWIP is to provide a strategic approach to identifying walking and cycling infrastructure improvements that are required at a local level.

Contextual information

6. In 2017, Government published the Cycling and Walking Investment Strategy (CWIS) with the ambition to “make cycling and walking the natural choice for

shorter journeys, or as part of a longer journey” by giving more access to safe, attractive routes for cycling by 2040. The strategy contains national targets to:

- increase the percentage of short journeys in towns and cities that are walked or cycled - half of all journeys in towns and cities cycled or walked by 2030;
 - increase walking;
 - double cycling; and
 - increase the percentage of children aged 5 to 10 who usually walk to school.
7. The strategy recommends that local highway authorities prepare new LCWIPs and the Government subsequently published technical guidance to support their preparation. The guidance notes that LCWIPs play a vital part in delivering the objectives of the CWIS, and to do this should deliver the following outputs:
- a network plan for walking and cycling which identifies preferred routes and core zones for further development;
 - a prioritised programme of infrastructure improvements for future investment; and
 - a report which sets out the underlying analysis carried out and provides a narrative which supports the identified improvements and network.
8. The process that is followed in developing LCWIPs is defined in the DfT's [Local Cycling and Walking Infrastructure Plan Technical Guidance](#). This document sets out a 6-stage process, which has been followed in preparing the Rushmoor LCWIP:
- determining scope- establishing the geographical extent and governance arrangements;
 - gathering information- Identifying existing patterns of, and barriers to walking and cycling potential new journeys;
 - network planning for cycling- Identifying origin and destination points and cycle flow and convert these into a network of routes with identified improvements;
 - network planning for walking- Identifying key trip generators, core walking zones and routes by undertaking audits and identifying improvements;
 - prioritising improvements- Prioritising improvements to develop a phased programme for future investment; and
 - integration and application- Integrate outputs into local planning and transport policies, strategies and delivery plans.
9. This process of developing the Rushmoor LCWIP was supplemented with a robust approach to consultation and engagement, following Hampshire County Council's usual practices. This includes stakeholder workshops and opportunities for members of the public to provide feedback.
10. The development of the Rushmoor LCWIP has also taken into account the latest Government guidance, including [Local Transport Note 1/20](#) (LTN 1/20). This technical guidance, published in 2020, provides Local Highway Authorities with guidance on the design of cycling infrastructure. Local Authorities should demonstrate that due consideration has been given to this guidance when designing new cycle infrastructure. This is of particular importance when

applying for Government funding, which may be withheld if the standards within LTN 1/20 have not been met.

Developing the Rushmoor LCWIP

11. The Rushmoor LCWIP was developed through a combination of desktop assessments, route audits across the district, stakeholder workshops and feedback from members of the public. This approach resulted in gaining a detailed picture of the existing state of the walking and cycling network in Rushmoor, with proposals for improved and new infrastructure informed by local stakeholders and residents who know the network best.
12. The information gathered allowed for the identification of a primary and secondary cycling network. The primary routes are judged to be the most popular and strategic routes, linking residential areas with the key trip generators. These are often the existing main roads in an area. Secondary routes can be locally important but are less strategic as they fill the gaps in the primary network, they are also often thought of as connector routes. Following the identification of 14 primary cycling routes, ten sections of cycling routes have been prioritised using the methodology outlined in the Rushmoor LCWIP. These are:
 - Old Lynchford Road to Lynchford Road, Farnborough;
 - Hawley Road, Farnborough;
 - A325 Bradfords Roundabout to Clockhouse Roundabout, Farnborough;
 - Station Road, Aldershot;
 - Williems Roundabout and Princes Way, Aldershot;
 - Chapel Lane, Farnborough;
 - Sandy Lane, Farnborough;
 - Mayfield Road, Farnborough;
 - Clockhouse Roundabout to Farnborough College of Technology, Farnborough; and
 - Government Road, Aldershot.
13. Similarly, the LCWIP identifies core walking zones. These are areas with several walking trip generators located in close proximity, such as town or local centres. Walking zones set out areas for investment in pedestrian infrastructure over a larger area rather than a specific route. Five core walking zones have been identified and a prioritisation process has been undertaken which identified the following priority sub sections for walking improvements:
 - Union Street and Wellington Street, Aldershot;
 - Victoria Road, Aldershot;
 - Princes Way and High Street, Aldershot;
 - Queens Road and Lime Street, Aldershot;
 - Sebastapol Road, Crimea Road and Pickford Street, Aldershot;
 - The Meads Shopping Centre, Farnborough;
 - Oberusel Way and Wingate Court Footpath, Aldershot;
 - Farnborough Town Centre to Farnborough Main via Clockhouse Roundabout, Farnborough;
 - Farnborough Industrial Estate to Farnborough Town Centre, Farnborough;
 - Cove Road to Farnborough town centre via Victoria Road, Farnborough; and
 - Station Road, Aldershot.

Finance

14. The development of the Rushmoor LCWIP has been undertaken in partnership with Rushmoor Borough Council, supported by consultants Sustrans. The total cost to develop the LCWIP is £96,000, which includes a contribution of £10,000 from the Borough Council.
15. In July 2022 the Government wrote to the Leaders of all Local Highway Authorities to inform them that changes were planned to the funding arrangement for transport. This included potentially withholding a proportion of highway maintenance funding depending on each authority's performance in developing a Local Transport Plan (LTP) and delivery of essential infrastructure for active travel and public transport, as well as electric vehicle charging facilities. The Government will consult on these proposals, with a view to implement changes to funding arrangements in 2024/25. LCWIPs form a key part of the evidence base for the LTP, therefore development of the Rushmoor LCWIP supports the case for a full funding settlement for Hampshire County Council.
16. In 2020 Government published [Gear Change](#), their vision to transform the role that walking and cycling play in England's transport system. The vision included the plan to invest two billion pounds in active travel infrastructure, and the establishment of a new active travel inspectorate, Active Travel England (ATE), to oversee standards and support local authorities to deliver quality plans and infrastructure.
17. Active Travel England (ATE) has a number of functions, including managing the national active travel budget, monitoring performance, providing training to local authorities and showcasing good practice in design, implementation and public engagement. As part of its role ATE recently conducted an audit of the capability and ambition of all Local Highway Authorities in England to develop and deliver high quality cycling infrastructure. Following this review Hampshire County Council was assessed as having "strong local leadership and support, with strong plans and emerging work".
18. The performance of each Local Highway Authority (LHA) in the audit was used by ATE to set the levels of funding that was allocated to each LHA through the Active Travel Capability Fund. Hampshire County Council was awarded £1,005,000 for 2022/23, which is three times higher than the funding the County Council was allocated in 2021/22, reflecting the positive feedback from ATE's audit of capability and ambition. The funding awarded will be used towards development of further LCWIPs, scheme development of active travel corridors (locations across the county to be confirmed), and behaviour change initiatives.

Performance

19. Delivery of the improvements identified within the Rushmoor LCWIP will be monitored on a regular basis to ensure that they are in line with current best practice and funding opportunities are leveraged. The LCWIP document will be reviewed in four to five years, or if there are any significant changes in local circumstances, to reflect any changes to the network due to new infrastructure delivery and to ensure that the document aligns with latest guidance.

20. In January 2023 central Government announced the projects that were successful in securing funding in the second round of the Levelling Up Fund. Rushmoor Borough Council was awarded £20 million for delivery of a leisure centre, library and cultural space that forms part of the Farnborough Civic Quarter redevelopment. As this project progresses opportunities will be sought to develop some of the walking and cycling infrastructure in Farnborough that has been identified in the Rushmoor LCWIP.

Consultation and Equalities

21. The Rushmoor LCWIP has been developed in partnership between Hampshire County Council and Rushmoor Borough Council, with support from consultants Sustrans. County Council and Borough Council officers have worked together closely to ensure that views at both a county and borough level have been considered.
22. In the early stages of developing the Rushmoor LCWIP workshops were held with key local stakeholders to gain insight into popular local walking and cycling routes as well as areas that could be improved as part of the future network. This stakeholder input was used to inform the development of the draft LCWIP which was issued for public consultation between 27 June and 18 September 2022.
23. The draft Rushmoor LCWIP was hosted on the 'Commonplace' website which allowed respondents to review the LCWIP document and add their comments to interactive maps. The maps enabled respondents to tell us how they felt using a particular route, as well as where they felt existing infrastructure could be improved. The Commonplace website also enabled respondents to complete separate walking zone and cycling network surveys. Over 600 individual responses to the draft Rushmoor LCWIP were provided through the Commonplace site, and a further 13 responses were provided via email from individuals, local groups, organisations or public bodies. There were also 74 responses to the walking zone survey and 75 responses to the cycling network survey.
24. The consultation surveys sought respondents' views on the proposed walking zones and cycling routes, including asking which zones and routes should be prioritised for investment and improvement. Respondents' views were supplemented by demographic questions, which were not mandatory.
25. During the consultation period an online briefing was held for Borough and County Members to provide them with an opportunity to learn more about the development of the Rushmoor LCWIP and to seek their feedback. Following the consultation period further briefings were held with Members and key stakeholders to update them on the progress with developing the LCWIP.
26. The feedback gathered through the engagement and consultation was used to prioritise the walking zones and cycling routes that were presented in the draft LCWIP document. Details on how the consultation responses and engagement feedback formed part of the assessment for the prioritisation of schemes can be found within the Consultation chapter (pages 195-201) of the [Rushmoor LCWIP document](#).
27. A neutral impact on people with protected characteristics has been identified from this decision. Whilst the Rushmoor LCWIP provides a policy basis for

improving relevant infrastructure, it does not however deliver change itself. Therefore, whilst any transport schemes that are identified in the LCWIPs are expected to have positive impacts on a range of protected characteristics such as age, disability, pregnancy & maternity, poverty and rurality by providing improved access and connectivity by non-motorised transport, they will be subject to their own Equalities Impact Assessment as schemes progress to delivery.

Climate Change Impact Assessments

28. Hampshire County Council utilises two decision-making tools to assess the carbon emissions and resilience of its projects and decisions. These tools provide a clear, robust, and transparent way of assessing how projects, policies and initiatives contribute towards the County Council's climate change targets of being carbon neutral and resilient to the impacts of a 2°C temperature rise by 2050. This process ensures that climate change considerations are built into everything the Authority does.
29. The preparation and adoption of the LCWIP will not, in itself, have any discernible impact on climate change, until any schemes are delivered, which will be assessed at an appropriate time. However, in due course, the delivery of walking and cycling schemes will potentially contribute to achieving targets on carbon reduction and will, ultimately and in combination, make a positive impact in helping the switch away from carbon-based transport modes.

Climate Change Adaptation

30. The climate change adaptation tool was not applicable to this report because the decision relates to a number of possible individual projects, which are subject to assessment individually at the point at which they come forward. The document itself is more strategic in nature and therefore doesn't have a direct impact on climate change.

Carbon Mitigation

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REQUIRED CORPORATE AND LEGAL INFORMATION:

Links to the Strategic Plan

Hampshire maintains strong and sustainable economic growth and prosperity:	yes
People in Hampshire live safe, healthy and independent lives:	yes
People in Hampshire enjoy a rich and diverse environment:	yes
People in Hampshire enjoy being part of strong, inclusive communities:	yes

Other Significant Links

Links to previous Member decisions:	
<u>Title</u> Decision - Local Cycling and Walking Infrastructure Plans – Update About the Council Hampshire County Council (hants.gov.uk)	<u>Date</u> 07/11/2022
Direct links to specific legislation or Government Directives	
<u>Title</u>	<u>Date</u>

Section 100 D - Local Government Act 1972 - background documents	
<p>The following documents discuss facts or matters on which this report, or an important part of it, is based and have been relied upon to a material extent in the preparation of this report. (NB: the list excludes published works and any documents which disclose exempt or confidential information as defined in the Act.)</p>	
<u>Document</u>	<u>Location</u>
None	

EQUALITIES IMPACT ASSESSMENT:

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- Advance equality of opportunity between persons who share a relevant protected characteristic within section 149(7) of the Act (age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation) and those who do not share it;
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- Take steps to meet the needs of persons sharing a relevant protected characteristic different from the needs of persons who do not share it;
- Encourage persons sharing a relevant protected characteristic to participate in public life or in any other activity which participation by such persons is disproportionately low.

2. Equalities Impact Assessment:

A neutral impact on people with protected characteristics has been identified from this decision. Whilst the Rushmoor LCWIP provides a policy basis for improving relevant infrastructure, however, it does not deliver change itself. Therefore, whilst any transport schemes that are identified in the LCWIPs are expected to have positive impacts on a range of protected characteristics such as age, disability, pregnancy & maternity, poverty and rurality by providing improved access and connectivity by non-motorised transport, they will be subject to their own Equalities Impact Assessment as schemes progress to delivery.

HAMPSHIRE COUNTY COUNCIL

Decision Report

Decision Maker:	Leader and Executive Member for Hampshire 2050 and Corporate Services
Date:	9 March 2023
Title:	Basingstoke Local Cycling and Walking Infrastructure Plan
Report From:	Assistant Chief Executive and Director of Hampshire 2050

Contact name: Sam Whitfield

Tel: 0370 7793 395

Email: Samuel.whitfield2@hants.gov.uk

Purpose of this Report

1. The purpose of this report is to seek approval for the Basingstoke & Deane Local Walking and Cycling Infrastructure Plan (LCWIP). It is also to set out the next steps for future study work and scheme delivery.

Recommendations

2. That the Leader and Executive Member for Hampshire 2050 and Corporate Services approves the Local Cycle and Walking Infrastructure Plan (LCWIP) that has been prepared for Basingstoke and Deane district.
3. That the Leader and Executive Member for Hampshire 2050 and Corporate Services commends the LCWIP to Basingstoke and Deane Borough Council for endorsement and inclusion within its own policy and plan framework.
4. That the Leader and Executive Member for Hampshire 2050 and Corporate Services confirms the top priority for developing active travel improvements in Basingstoke relates to the town centre and that improvements need to be progressed in conjunction with the Basingstoke Town Centre Regeneration proposals currently led by the Borough Council.

Executive Summary

5. This paper seeks to outline the work that has been undertaken in preparing the Basingstoke and Deane LCWIP and seeks approval of the LCWIP document as part of the transport strategy for Basingstoke and Deane Borough.
6. The Basingstoke LCWIP was developed in partnership between Hampshire County Council and Basingstoke and Deane Borough Council, with support from consultants Sustrans, following the latest government guidance. The purpose of the LCWIP is to provide a strategic approach to identifying walking and cycling infrastructure improvements within the borough that are required at a local level.

7. The LCWIP document can be found at this link:
<https://documents.hants.gov.uk/transport/Basingstoke-and-Deane-LCWIP-Report.pdf>

Contextual information

8. In 2017 the Government published a Cycle and Walking Investment Strategy with the “ambition to make cycling and walking the natural choices for shorter journeys, or as part of a longer journey by 2040”. It set national targets to:
- increase the percentage of short journeys in towns and cities that are walked or cycled - half of all journeys in towns and cities cycled or walked by 2030;
 - increase walking;
 - double cycling; and
 - increase the percentage of children aged 5 to 10 who usually walk to school.
9. The strategy recommended that new LCWIPs be prepared by local highway authorities. The Government subsequently published technical guidance for local authorities on the preparation of LCWIPs requiring that they should include:
- a network plan for walking and cycling which identifies preferred routes and core zones for further development;
 - a prioritised programme of infrastructure improvements for future investment in the short, medium and long terms; and
 - a report which sets out the underlying analysis carried out and provides a narrative which supports the identified improvements and network.
10. The technical guidance sets out further detail on the process that should be followed to develop LCWIPs ([Local Cycling and Walking Infrastructure Plan Guidance](#)). It prescribed a six-stage process involving scoping the geographical extent, data gathering, auditing of existing routes, identifying improvements required, prioritising and programming improvements and promoting schemes in local policies and delivery plans. It also requires a robust approach to consultation and engagement. In developing LCWIPs in Hampshire the guidance has been followed.
11. In 2020, Government published Local Transport Note (LTN) 1/20 (Cycle infrastructure design (LTN 1/20) - GOV.UK (www.gov.uk)) providing national guidance on design for cycling infrastructure for highway authorities and scheme designers. The guidance marks a step change in achieving higher quality design standards and improved safety for cycle facilities. The Department for Transport also reserved the right to ask for appropriate funding to be withheld or returned for any schemes designed or built in a way which is not consistent with the guidance.
12. “Gear Change”, published by the Government in 2020, announced a plan to invest two billion pounds in active travel infrastructure and set out a plan to

establish a new active travel inspectorate called Active Travel England (ATE) to oversee standards and support local authorities to deliver quality plans and infrastructure.

13. Active Travel England has now been established by Government to manage the national active travel budget and to inspect and publish reports on highway authorities for their performance. ATE will help local authorities, training staff and spreading good practice in design, implementation and public engagement. It will also be a statutory consultee on major planning applications to ensure that the largest new developments are properly catering for pedestrians and cyclists.
14. ATE recently carried out an audit of the capability and ambition of all Highway Authorities in England with regard to cycle infrastructure. Hampshire County Council has been assessed as level 2, which is described as “strong local leadership and support, with strong plans and emerging work”.
15. Hampshire County Council has recently been awarded £1,005,000 of Department for Transport (DfT) Active Travel Capability funding (out of £32.9 million distributed across England). The award is 3 times higher than that for 2021/22 and demonstrates that ATE has confidence that Hampshire County Council has the capability and ambition to deliver high quality active travel infrastructure and initiatives. The funding will be used (amongst other items) towards the development of LCWIPs, scheme development of up to 8 urban active travel corridors, and public engagement. £205,000 of the funding is committed towards behaviour change initiatives.

Completed LTN 1\20 compliant Schemes in Basingstoke

16. A new LTN 1/20 complaint cycle route was completed between the Brighton Way roundabout and Sullivan Road in April 2022. This involved the improvement of the existing footpath and a new 3m wide segregated cycle track, which is separated from the footpath by a kerb. This will form part of one of the secondary routes identified through the Basingstoke LCWIP heading south from the Brighton Way roundabout.
17. This scheme, which was completed using money secured through the Active Travel Fund, will tie into the ongoing improvements at Brighton Hill roundabout for improved cyclist provision. The Brighton Hill roundabout is identified as key crossover point for multiple primary and secondary cycling routes in the LCWIP. Further details on the Brighton Way scheme can be found here: [Active Travel Fund; Brighton Way Cycle Improvements | Transport and roads | Hampshire County Council \(hants.gov.uk\)](#). The cycle track was the first LTN 1/20 compliant infrastructure scheme delivered in Basingstoke.

Development of the Basingstoke LCWIP

18. Work to develop the Basingstoke LCWIP commenced in spring 2021. The LCWIP was funded by Hampshire County Council and Basingstoke and

Deane Borough Council. This LCWIP has been co-developed by the County Council and Sustrans with active support from the Borough Council.

19. The draft LCWIP was drawn up to look at cycle routes and walking zones within the borough, and in some cases linking into key settlements in neighbouring areas. Through a series of workshop events and analysis of supporting data a network of primary and secondary cycle routes in Basingstoke was identified. The primary routes are judged to be the most popular and strategic routes, linking residential areas with the key trip generators. Secondary routes can be locally important but are less strategic as they fill the gaps in the primary network. The primary cycling routes identified are:
- Route 110: Basingstoke to Manydown and Hatch Warren;
 - Route 120: Tadley to Basingstoke;
 - Route 130: Basingstoke to Harrow Way;
 - Route 140: Sherfield-on-Loddon to Basingstoke;
 - Route 210: Manydown to Basingstoke Town Centre;
 - Route 220: Basingstoke to Overton and Whitchurch;
 - Route 230: Lychpit to Eastern Basingstoke;
 - Route 240: Hook to Basingstoke;
 - Route 250: Viables to Oakley; and
 - Route 260: Dummer to Basingstoke.
20. Similarly, the LCWIP identifies core walking zones. These are areas with several walking trip generators located in close proximity, such as town or local centres. Walking zones set out areas for investment in pedestrian infrastructure over a larger area rather than a specific route. Core walking zones have been identified and a prioritisation process has been undertaken which identified the following priority sub sections for walking improvements:
- Whitchurch Core Walking Zone;
 - Overton Core Walking Zone;
 - Tadley Core Walking Zone; and
 - Basingstoke Core Walking Zone (to be considered separately).

Basingstoke Town Centre

21. On 6 December 2022, Basingstoke and Deane Borough Council adopted its Town Centre Masterplan which sets out a vision for regenerating the town to create a more sustainable, accessible and thriving place fit for the future. Further details can be found at [Shaping the future of Basingstoke town centre](#). As the Borough Council's work on the Masterplan was being undertaken prior to the development of the LCWIP, it was considered duplication to include detailed assessments of the walking zones and cycling routes in Basingstoke

Town Centre as part of the LCWIP. It was therefore omitted from the LCWIP assessment.

22. The transport proposals in the Basingstoke Town Centre Masterplan include a new movement and access strategy which aligns well with improving walking and cycling infrastructure.
23. The Masterplan proposes to prevent the movement of through-traffic in the town centre by creating a new model of mobility for the town centre. This is based on developing a number of traffic access zones, that prevent the movement of private vehicles from traversing through the town centre and instead direct through movements around the Ringway. The traffic access zones would allow priority access between them for active modes, public transport and other priority vehicles, achieved by introducing modal filters. By applying this approach of modal filters, it promotes bus priority and walking and cycling through the town core but reduces vehicle traffic levels significantly. Private vehicles would not be prevented from driving to the Town Centre, but they would be required to access and leave the Town Centre from the same Ringway entrance and exit point. Such interventions would lead to lower levels of traffic in the town centre and provide a strong framework around which to design future cycling, walking and public transport improvements.
24. Achieving better active travel access through and within the town centre is key to driving up the mode share of active travel journeys in the whole of Basingstoke town. This is because the town centre currently presents a significant barrier to walking and cycling. The LCWIP has not included the Town Centre for the reasons mentioned above and because of that it does not identify any priorities for action in the town centre. In order to recognise the fact that the most impactful schemes will be in the town centre this report recommends that in association with adopting the LCWIP it is recognised that the top priority for improvement is the town centre area.
25. The regeneration proposals for the town centre have rightly been developed by Basingstoke and Deane Borough Council. The County Council has responded with technical advice and support but moving forward there is a case for the County Council to take a more active lead on developing the movement plan for the town centre. If the plan could come forward first there is potential for it to be a catalyst for the regeneration ambition of the Borough. This lends further support to the recommendation that the town centre be assigned the highest priority for further work.

Tadley Walking and Cycling Improvements

26. Using the methodology for assessing walking zones that is set out in the Basingstoke LCWIP, Tadley has been identified as one of four core walking zones across the borough. The interventions identified in the LCWIP to improve walking facilities in the Tadley core walking zone correspond with the findings of a feasibility study that was undertaken of the area. The study was commissioned to review the walking and cycling interventions that could be made in Tadley to help mitigate the transport impacts of development in the area.

27. The study identified possible interventions that would require around £2million if they were all delivered. At present there is £200,187 in developer contributions held for transport improvements in Tadley, therefore the Basingstoke LCWIP will be used to inform the prioritisation of the use of the developer contributions within the Tadley core walking zone, taking into account the options identified in the feasibility study.

Finance

28. The development of the Basingstoke LCWIPs has largely been done in conjunction with Basingstoke and Deane Borough Council as a jointly funded project, with Sustrans contracted to undertake some of the work. The scheme costs to date are £66,925, with a £30,000 contribution by the Borough Council.

29. The Government wrote to all Highway Authority Council Leaders in July 2022. The letter indicated that the Government was planning changes to future funding arrangements for transport, potentially including withholding a proportion of highways maintenance funding depending on the authority's performance on effective development of an LTP and delivery of EV charging facilities and bus and active travel infrastructure. The letter says that the Government will consult on the proposals with a view to implementation from 2024/25 onwards. A strong evidence base, of which LCWIPs will form an important part, will be key to delivering against these objectives and thereby minimising the risk of reductions in future maintenance funding.

30. In terms of developer contribution funding available for Tadley, there is £200,187 available for interventions. There is no limitation on the date by which these contributions have to be spent.

Consultation and Equalities

31. The draft LCWIP for Basingstoke and Deane has been subject to public consultation, between 20 June and 4 September 2022. Separate surveys were held for the walking and cycling elements, with 192 responses received to the cycling survey and 55 responses received for the walking survey.

32. The consultation used 'Commonplace' an interactive map enabling 'sentiment' maps (how people felt using a particular route) and 'off route' maps (where respondents felt that walking or cycling infrastructure could be improved) to be produced for each area. There were 573 responses on the online maps, with a further 417 'agreements' with these comments.

33. An online briefing was held for both County and Borough councillors at the start of the consultation period. Moreover, both organisations wanted to ensure that the consultation reached as many residents as possible. Beyond the social media adverts, County Council officers set up pop-up events in Basingstoke town centre on 17 and 18 August 2022, where town centre visitors could ask about the LCWIP development. Leaflets with a QR code and website link were also handed out and individuals were assisted to complete the surveys. Some stakeholders also returned further comments via email.

34. The consultation surveys asked respondents whether they agreed with the proposed walking and cycling routes, and to prioritise the routes and zones that they would like to see come forward first. Respondents were also asked about the barriers to walking and cycling in their local area and why they walked or cycled for their journeys. Demographic data was also collected allowing an analysis of who was responding to the consultation.
35. Further detail on the consultation responses can be found within the Consultation (pages 111-122) chapter of the [LCWIP document](#).
36. Basingstoke and Deane Borough Council officers have actively been involved in the development of this LCWIP. Individual councillors and officers also submitted feedback via the consultation process.
37. Moving forward, the feedback from the consultation will be used for the purposes of understanding key priorities in Basingstoke town centre, and to demonstrate public support for funding opportunities. It will also be used to inform the design of schemes and to identify future walking zones and links in the cycling network.
38. A neutral impact on people with protected characteristics has been identified from this decision. Whilst the Basingstoke LCWIP provides a policy basis for improving relevant infrastructure, it does not however deliver change itself. Therefore, whilst any transport schemes that are identified in the LCWIPs are expected to have positive impacts on a range of protected characteristics such as age, disability, pregnancy & maternity, poverty and rurality by providing improved access and connectivity by non-motorised transport, they will be subject to their own Equalities Impact Assessment as schemes progress to delivery.

Climate Change Impact Assessments

39. Hampshire County Council utilises two decision-making tools to assess the carbon emissions and resilience of its projects and decisions. These tools provide a clear, robust, and transparent way of assessing how projects, policies and initiatives contribute towards the County Council's climate change targets of being carbon neutral and resilient to the impacts of a 2°C temperature rise by 2050. This process ensures that climate change considerations are built into everything the Authority does. The application of these tools (the Climate Change Adaption and Carbon Mitigation tools) is considered below.
40. The preparation and adoption of the LCWIP will not, in itself, have any discernible impact on climate change, and neither will the scheme development for Tadley until the scheme is delivered, which will be assessed at an appropriate time. However, in due course, the delivery of walking and cycling schemes will potentially contribute to achieving targets on carbon reduction and will, ultimately and in combination, make a positive impact in helping the switch away from carbon-based transport modes.

Climate Change Adaptation

41. The climate change adaptation tool was not applicable to this report because the decision relates to a number of possible individual projects, which are subject to assessment individually at the point at which they come forward. The document itself is more strategic in nature and therefore doesn't have a direct impact on climate change.

Carbon Mitigation

42. The climate change mitigation tool was not applicable to this report because the decision relates to a number of possible individual projects, which are subject to assessment individually at the point at which they come forward. The document itself is more strategic in nature and therefore there are no clear mitigation measures which could be taken forward at this time.

REQUIRED CORPORATE AND LEGAL INFORMATION:

Links to the Strategic Plan

Hampshire maintains strong and sustainable economic growth and prosperity:	Yes
People in Hampshire live safe, healthy and independent lives:	Yes
People in Hampshire enjoy a rich and diverse environment:	Yes
People in Hampshire enjoy being part of strong, inclusive communities:	Yes

Other Significant Links

Links to previous Member decisions:	
<u>Title</u>	<u>Date</u>
Decision - Local Cycling and Walking Infrastructure Plans – Update About the Council Hampshire County Council (hants.gov.uk)	07/11/2022
Decision Record - Basingstoke Transport Update-2022-10-03-ELMTES Decision Day (hants.gov.uk)	03/10/2022
Direct links to specific legislation or Government Directives	
<u>Title</u>	<u>Date</u>

Section 100 D - Local Government Act 1972 - background documents	
<p>The following documents discuss facts or matters on which this report, or an important part of it, is based and have been relied upon to a material extent in the preparation of this report. (NB: the list excludes published works and any documents which disclose exempt or confidential information as defined in the Act.)</p>	
<u>Document</u>	<u>Location</u>
None	

EQUALITIES IMPACT ASSESSMENT:

1. Equality Duty

The County Council has a duty under Section 149 of the Equality Act 2010 ('the Act') to have due regard in the exercise of its functions to the need to:

- Eliminate discrimination, harassment and victimisation and any other conduct prohibited by or under the Act with regard to the protected characteristics as set out in section 4 of the Act (age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation);
- Advance equality of opportunity between persons who share a relevant protected characteristic within section 149(7) of the Act (age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation) and those who do not share it;
- Foster good relations between persons who share a relevant protected characteristic within section 149(7) of the Act (see above) and persons who do not share it.

Due regard in this context involves having due regard in particular to:

- The need to remove or minimise disadvantages suffered by persons sharing a relevant characteristic connected to that characteristic;
- Take steps to meet the needs of persons sharing a relevant protected characteristic different from the needs of persons who do not share it;
- Encourage persons sharing a relevant protected characteristic to participate in public life or in any other activity which participation by such persons is disproportionately low.

2. Equalities Impact Assessment:

A neutral impact on people with protected characteristics has been identified from this decision. Whilst the Basingstoke LCWIP provides a policy basis for improving relevant infrastructure, it does not however deliver change itself. Therefore, whilst any transport schemes that are identified in the LCWIPs are expected to have positive impacts on a range of protected characteristics such as age, disability, pregnancy & maternity, poverty and rurality by providing improved access and connectivity by non-motorised transport, they will be subject to their own Equalities Impact Assessment as schemes progress to delivery.

HAMPSHIRE COUNTY COUNCIL

Decision Report

Decision Maker:	Leader and Executive Member for Hampshire 2050 and Corporate Services
Date:	9 March 2023
Title:	Welborne Street Manual
Report From:	Assistant Chief Executive and Director of Hampshire 2050

Contact name: Laura McCulloch

Tel: 03707 793886

Email: Laura.mcculloch@hants.gov.uk

Purpose of this Report

1. The purpose of this report is to summarise what the Welborne Street Design Manual is, its purpose, how it has been drafted and reviewed and to seek approval and adoption of the Welborne Street Design Manual once approved via discharge of Condition 10 of the Welborne planning permission P/17/0266/OA.

Recommendations

2. That the Leader and Executive Member for Hampshire 2050 and Corporate Services approve the Welborne Street Manual following discharge of Condition 10 of the Welborne planning permission P/17/0266/OA; and
3. That the Leader and Executive Member for Hampshire 2050 and Corporate Services adopt the Welborne Street Manual following discharge of Condition 10 of the Welborne planning permission P/17/0266/OA.

Executive Summary

4. Welborne is a large development north of Fareham. Planning permission has been granted under reference P/17/0266/OA and the developer is currently discharging pre-commencement planning conditions to allow construction to start.
5. The development consists of a new community of up to 6,000 dwellings (including a care home) together with a district centre and a village centre, up to 30,000sqm of commercial and employment space, up to 35,000sqm of general industrial use, up to 40,000sqm of warehousing space, a hotel, community uses, a nursery, a health centre and veterinary services, a secondary school, three primary schools, pre-schools, green infrastructure, a household waste recycling centre, remodelled of the M27 J10 junction, works to the A32 including the creation of three highway junctions and new crossings, distributor roads (accommodating a Bus Transit network) and connections to the surrounding cycleway and pedestrian network.

6. Condition 10 of the Welborne planning permission states the following:

Prior to the approval of the First Reserved Matters Application, a Street Design Manual shall be submitted to and approved in writing by the Local Planning Authority. This Street Design Manual shall include:

- *Street Design Principles for the street network*
- *General Street Design Principles for the other internal road network*
- *Timescales for the delivery of the primary street network*
- *Adoption Strategy*
- *Parameters and details of the main north to south route through the site, including link capacity, frontage activity, on-street parking provision / restriction, adoption strategy, timescale for delivery, number of junctions, minimum centreline radii, footway details, cycleway details, public transport / BRT details, typical plans, typical sections and typical junction arrangements*

This Street Design Manual will be substantially in conformance with the approved Structuring Plan. Any variations to this Design Manual must first be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out substantially in accordance with the approved details.

REASON: In the interest of a suitable carrying capacity being provided on the internal road network and to ensure a comprehensive and appropriate form of development

7. The Welborne Street Manual sits alongside the Design Guide which together will be key guides and design frameworks for housebuilders and infrastructure providers to follow, ensuring the vision for Welborne remains cohesive and undiluted through the lifetime of the build. The Welborne Street Manual will include well-thought through street cross sections that give clarity to the role and function of all areas of the public domain. It will illustrate how the design of streets and spaces can fulfil their potential to add value to the various character areas and are driven by the requirements of the plots they serve.

8. Officers have been working with the Welborne development team since 2018 to draft and agree the principles of the Welborne Street Design Manual as set out above. This has involved working collaboratively across directorates, with input provided by the following teams:

- Highway Development Planning;
- Spatial Planning;
- Hampshire Engineering Services;
- Asset Management;
- ITS and Street Lighting;
- Flood Water Management;
- Highway Development Client;
- Hampshire Legal Service;

- Implementation; and
 - Arboriculture.
9. Following this work, the principles of the Welborne Street Design Manual are agreed and the Welborne developer will shortly submit the completed Welborne Street Design Manual to Fareham Borough Council with the aim of discharging Condition 10 of the Welborne Planning Permission. Hampshire County Council will be consulted on this submission and the teams above will have a further opportunity to comment on Welborne Street Design Manual. Following any final amendments (if required), Condition 10 will be discharged, and the Welborne Street Design Manual will be set as the highway design guidance, informing the design principles for the internal road network of the Welborne development. The Welborne Street Design Manual (appended to this report) includes periodic review, with the opportunity for amendments to ensure highway design standards and principles remain current during the forecast c.20-year construction period of the development.
 10. Working in partnership, Fareham Borough Council, the Welborne developer and Hampshire County Council are keen that the Welborne Street Design Manual has sufficient status to enable it to influence the way Welborne as a place is shaped for the longevity of the development construction. As such it was considered appropriate to seek approval from the Leader and Executive Member for Hampshire 2050 and Corporate Services, signalling the importance that the document has to place shaping in the largest new community to be planned in Hampshire for many years. Approval at this level will ensure that teams involved in the detailed review of the internal road design through future Reserved Matters and section 38 (Road Adoption) submissions are in accordance with the Welborne Street Design Manual and achieve the vision that it sets out.

Finance

11. Review of the Welborne Street Manual to date has been funded by the developer through the Highways Development Planning Pre-Application Service. Going forward, adoption of the Welborne Street Design Manual has no financial implications for Hampshire County Council over and above the Authority's statutory duty to respond to consultations from Fareham Borough Council as Local Planning Authority. Review of future section 38 submissions and legal agreements will be funded by the developer through existing processes and procedures.

Consultation and Equalities

12. Due to the nature of the approval for this report, limited consultation has been undertaken. However, the public has been engaged in the development of proposals at Welborne through the local plan and planning application processes. The Street Manual, when submitted to the Local Planning Authority to discharge Condition 10, will be subject to public consultation.
13. The decision sought in this report will not reduce the scope of the service provided or have any impact on service users or the individuals working on the service, so has been assessed as having a neutral impact on groups with

protected characteristics. In the development of the Street Manual, and subsequently the detailed design proposals of the development as it comes forward, the Local Planning Authority and Local Highway Authority consider accessibility, safety and useability of the public realm in order to ensure it will have a positive impact on all users.

Climate Change Impact Assessments

14. Hampshire County Council utilises two decision-making tools to assess the carbon emissions and resilience of its projects and decisions. These tools provide a clear, robust, and transparent way of assessing how projects, policies and initiatives contribute towards the County Council's climate change targets of being carbon neutral and resilient to the impacts of a 2°C temperature rise by 2050. This process ensures that climate change considerations are built into everything the Authority does.
15. Approval of the Welborne Street Design Manual will have a no impact on climate change itself, however climate change adaptation and mitigation is a key consideration for the Local Planning Authority and Local Highway Authority in assessing the development proposals. The inclusion of sustainable drainage, trees to provide shading, and priority for walking and cycling within the development design are examples of this.

Climate Change Adaptation

16. Approval of the Welborne Street Design Manual will have a no impact on climate change itself, however climate change adaptation and mitigation is a key consideration for the Local Planning Authority and Local Highway Authority in assessing the development proposals. The inclusion of sustainable drainage, trees to provide shading, and priority for walking and cycling within the development design are examples of this.

Carbon Mitigation

17. Approval of the Welborne Street Design Manual will have a no impact on carbon mitigation, which will be considered in any event through future Reserved Matters and S38 (Road Adoption) submissions.

Conclusions

18. Approval of the Welborne Street Design Manual is considered beneficial to ensure there is a consistent approach over the construction life of the Welborne development, across Directorates and by all teams within Hampshire County Council involved in detailed review of the internal road design through future Reserved Matters and S38 (Road Adoption) submissions, and to ensure that these future reviews and approvals are in accordance with the Welborne Street Design Manual.

REQUIRED CORPORATE AND LEGAL INFORMATION:

Links to the Strategic Plan

Hampshire maintains strong and sustainable economic growth and prosperity:	yes
People in Hampshire live safe, healthy and independent lives:	yes
People in Hampshire enjoy a rich and diverse environment:	yes
People in Hampshire enjoy being part of strong, inclusive communities:	yes

Section 100 D - Local Government Act 1972 - background documents

The following documents discuss facts or matters on which this report, or an important part of it, is based and have been relied upon to a material extent in the preparation of this report. (NB: the list excludes published works and any documents which disclose exempt or confidential information as defined in the Act.)

Document

Location

None

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1. Equality Duty

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- Eliminate discrimination, harassment and victimisation and any other conduct prohibited by or under the Act with regard to the protected characteristics as set out in section 4 of the Act (age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation);
- Advance equality of opportunity between persons who share a relevant protected characteristic within section 149(7) of the Act (age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation) and those who do not share it;
- Foster good relations between persons who share a relevant protected characteristic within section 149(7) of the Act (see above) and persons who do not share it.

Due regard in this context involves having due regard in particular to:

- The need to remove or minimise disadvantages suffered by persons sharing a relevant characteristic connected to that characteristic;
- Take steps to meet the needs of persons sharing a relevant protected characteristic different from the needs of persons who do not share it;
- Encourage persons sharing a relevant protected characteristic to participate in public life or in any other activity which participation by such persons is disproportionately low.

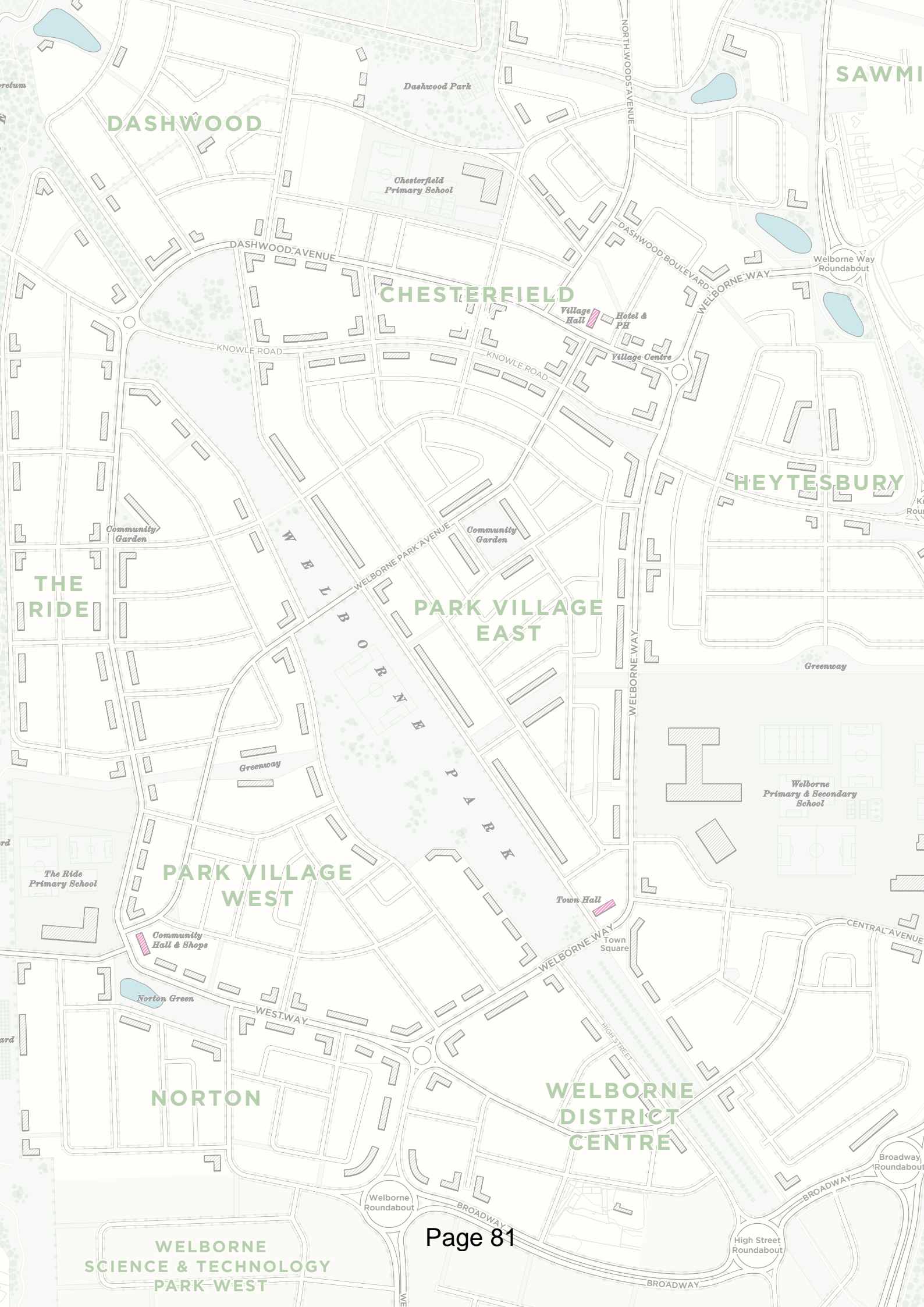
2. Equalities Impact Assessment:

The decision sought in this report will not reduce the scope of the service provided or have any impact on service users or the individuals working on the service, so has been assessed as having a neutral impact on groups with protected characteristics. In the development of the Street Manual, and subsequently the detailed design proposals of the development as it comes forward, the Local Planning Authority and Local Highway Authority consider accessibility, safety and useability of the public realm in order to ensure it will have a positive impact on all users.

WELBORNE

STREETS MANUAL

EDITION 1



DASHWOOD

Dashwood Park

Chesterfield Primary School

CHESTERFIELD

Village Hall

Hotel & PH

Village Centre

DASHWOOD AVENUE

KNOWLE ROAD

KNOWLE ROAD

DASHWOOD BOULEVARD

WELBORNE WAY

Welborne Way Roundabout

HEYTESBURY

Community Garden

THE RIDE

WELBORNE PARK AVENUE

Community Garden

PARK VILLAGE EAST

Greenway

Greenway

Welborne Primary & Secondary School

PARK VILLAGE WEST

The Ride Primary School

Community Hall & Shops

Town Hall

Town Square

CENTRAL AVENUE

Norton Green

WESTWAY

WELBORNE WAY

NORTON

WELBORNE DISTRICT CENTRE

HIGH STREET

Broadway Roundabout

Welborne Roundabout

BROADWAY

BROADWAY

High Street Roundabout

WELBORNE SCIENCE & TECHNOLOGY PARK WEST

1. PREFACE

Welborne is envisioned as a Garden Village for the twenty-first century. Grounded upon the principles of the Garden City movement of the early twentieth century, the vision for Welborne is to create a holistically planned, characterful and self-sustaining addition to Britain's rich legacy of new communities. Welborne will be a sustainable new settlement combining housing, shops, schools, local facilities and employment within an attractive, walkable and leafy environment realised to high standards of design and construction. It will be a place where people can work, socialise, experience green spaces and enjoy a good quality of life.

The coding documents will set out the identity, character and central vision to ensure it is maintained during the building process and for future generations.

The suite of coding documents consists of the Strategic Design Code, the Welborne Streets Manual and the Neighbourhood Design Codes, which will be produced to guide the individual neighbourhoods as they are brought forward.

The role of each coding document is as follows:

- **Strategic Design Code:** Sets the principles for the Welborne vision and the site-wide framework that will enable it to be achieved.
- **Welborne Streets Manual:** Outlines the regulations that inform the street network design.
- **Neighbourhood Design Codes:** Provides detailed guidance on street, block, building and landscape design specific to the individual neighbourhoods. The Codes are informed by the Strategic Design Code and Welborne Streets Manual, and will include a compliance checklist as a simple way to verify that the guidance is adhered to.

Overseen and enacted by the Master Developer, Town Architect and Fareham Borough Council the coding documents will provide a framework to ensure that Welborne is well planned, designed to a quality in accordance with the vision and built to last.

STRATEGIC DESIGN CODE

- 1. An introduction to Welborne**
 - Vision
 - Illustrative masterplan
 - Neighbourhood structure
- 2. Explaining the Strategic Design Code**
 - Its objectives and how to use the Code
- 3. Strategic masterplans and town-wide regulations**
- 4. Landscape**
- 5. Character elements**
 - The design elements that will shape Welborne's character
- 6. Neighbourhoods**
 - Characteristics and key components of each neighbourhood
- 7. Technical principles**
 - Site-wide principles that apply to all neighbourhoods

WELBORNE STREETS MANUAL

- 1. Introduction to the Welborne Streets Manual**
- 2. Explaining the Welborne Streets Manual**
- 3. Placemaking Principles**
- 4. Adoption, Management and Maintenance**
 - Role of Welborne Garden Village Trust
- 5. Movement Strategy**
- 6. Street Design Principles**
 - How streets will look, feel and function
- 7. Street layout**
 - Typical street, junction and driveway types
 - Special places

NEIGHBOURHOOD DESIGN CODES

- 1. Explaining the Neighbourhood Design Code**
 - Its objectives and how to use the Code
- 2. Neighbourhood context**
- 3. Illustrative plan**
 - How the neighbourhood will be brought forward
- 4. Neighbourhood layout**
- 5. Landscape strategy**
- 6. Built form**
 - Design requirements relating to appearance, scale, materials and details of buildings
- 7. Points of delight**
- 8. Key infrastructure requirements**
 - Schools etc
- 9. Applying the design principles**
 - Guidance for code-compliant design
- 10. Property owners guidance**
 - For owners wishing to carry out development or alterations
- 11. Compliance checklist**

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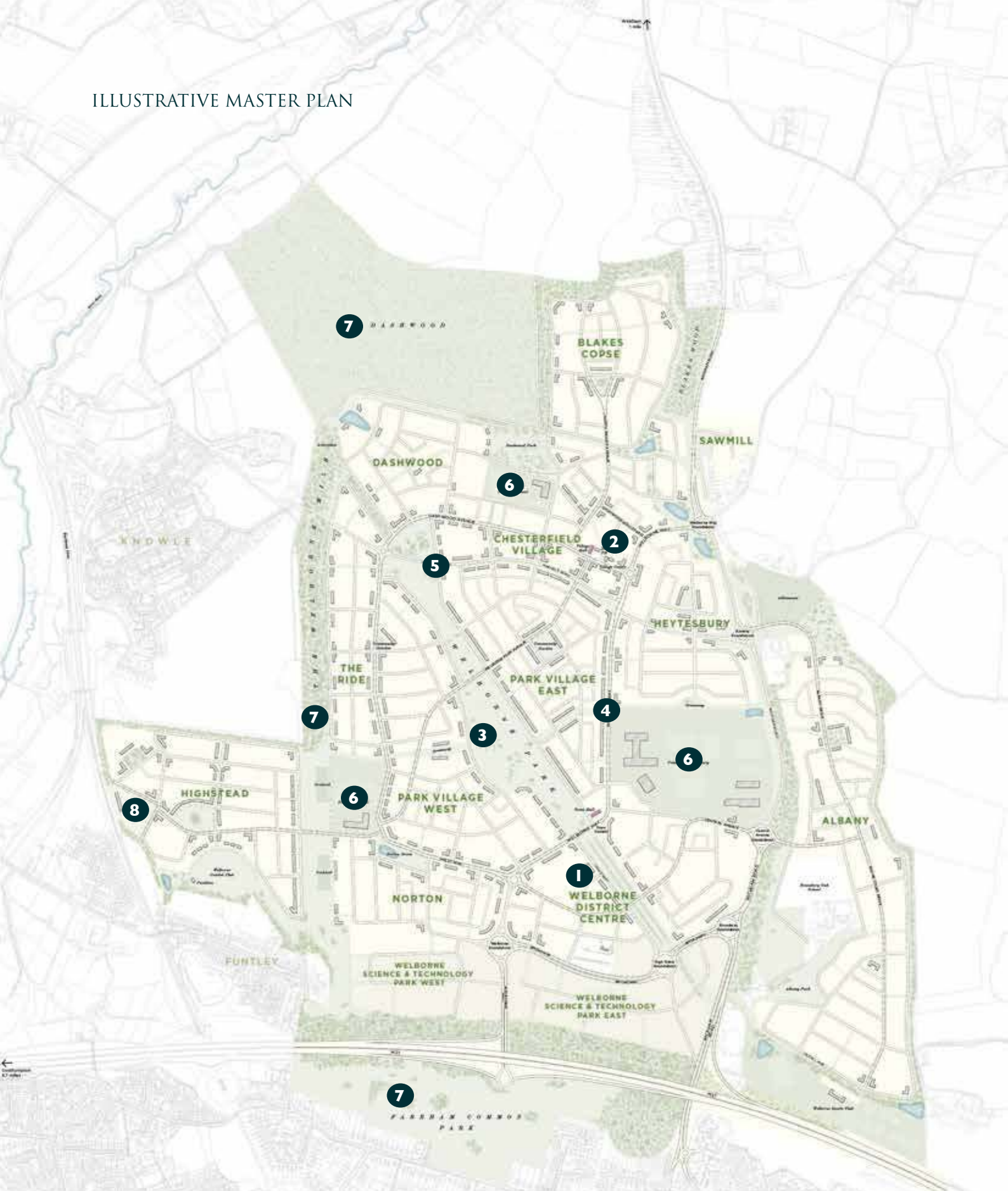
2. INTRODUCTION

Streets make up around 80 per cent of the public realm in our villages, towns and cities. They are the lifeblood of these places, bringing vibrancy and movement to a whole range of spaces and places, from town centres to villages greens, avenues, parks and quieter places. They are the 'glue' that hold our settlements together and are the conduits for good movement, for creating real communities and great places.

Far too often, though, the highway engineering of street design and the desire to accommodate motorists can dominate the placemaking elements of a street. In recent years across the United Kingdom, new streets have been built where vehicles are prioritised over pedestrians and cyclists. Such roads are a less healthy way to build and do not foster walkability and community spirit.

This is not to be the case for Welborne. Its streets will be built at a human scale with walking, cycling and the use of public transport prioritised to build an inclusive, beautiful, twenty-first-century new community. The Welborne Streets Manual has been developed through a series of collaborative workshops with Fareham District Council and Hampshire County Council to set out the regulations and principles that will govern the development of the street network. Its aim is to allow for the delivery and adoption of high-quality, tree- and hedge-lined streets within the new settlement that support a comprehensive and appropriate form of development.

ILLUSTRATIVE MASTER PLAN



KEY

- | | | | | | |
|----------|-----------------|----------|--------------|----------|-------------------------|
| 1 | District Centre | 4 | Welborne Way | 7 | SANGs |
| 2 | Village Centre | 5 | Knowle Road | 8 | Rail halt (if required) |
| 3 | Welborne Park | 6 | Schools | | |

2a. WELBORNE STREETS MANUAL: OVERVIEW

The Welborne Streets Manual sets out the regulations that will govern the development of the street network. It will facilitate specific outcomes through clear guidance and clarity of design. It will elucidate the urban and landscape tenets that make up the streetscapes, to make sure that all phases of construction come together cohesively to facilitate the delivery of the tree-lined, human-scaled streets that are integral to the Welborne vision. The Welborne Streets Manual covers the design parameters of all roads, parking, public and private footpaths and cycleways, excluding M27 J10.

The design guidance within the Welborne Streets Manual is specific to Welborne and reflects best practice urban design guidance including the National Planning Policy Framework, Planning Practice Guidance (incorporating the National Design Guide), Manual for Streets 1 and 2, Active Design and Building for a Healthy Life.

The Welborne Streets Manual also considers and responds to policies set out in the Welborne Plan and Welborne Design Guidance, including the Residential Car and Cycle Parking Standards Supplementary Planning Document 2009 and the non-Residential Car and Cycle Parking Standards Supplementary Planning Document 2015.



A typical Welborne tree-lined street

KEY STAKEHOLDERS AND AUTHORITY

Fareham Borough Council

Fareham Borough Council (FBC) will approve the Welborne Streets Manual and all other Welborne coding documentation. The coding documents will be a material consideration for any planning application at Welborne and applications for development at Welborne will be expected to demonstrate code compliance; this will be shown by fulfilling the compliance checklists found within each Neighbourhood Design Code.

Hampshire County Council

Hampshire County Council will utilise the Welborne Streets Manual when assessing the design of highways across Welborne to ensure proposals are in accordance with the agreed principles and vision.

Master Developer

Buckland is the Master Developer and will lead the development of Welborne. The Master Developer will work with all development partners throughout the lifetime of the project and alongside the Town Architect will be the custodians of the values, vision and quality of Welborne.

Town Architect

Buckland have appointed Ben Pentreath as the Town Architect. The Town Architect will review proposals for residential, commercial and public realm schemes with reference to the Welborne Streets Manual alongside the Strategic and Neighbourhood Design Codes. The Town Architect will also approve housebuilders' construction drawings and monitor the built output as each phase is completed.

Welborne Garden Village Trust

The Welborne Garden Village Trust (WGVT) is a not-for-profit community organisation that is responsible for the long-term stewardship of the Garden Village. All unadopted areas of the development will be transferred to WGVT which will be responsible for the management and maintenance of these areas in perpetuity.

The WGVT is also responsible for upholding the masterplan, characteristics and quality of the Garden Village as set out in the coding documentation.

Governance

The Master Developer will administer and implement the Welborne Streets Manual. Any development proposal brought forward for Welborne will be approved by the Master Developer and Town Architect prior to any submission of reserved matters applications to FBC. Specific highway approvals will be dealt with by HCC under Section 38 (S38) agreements or other highways approval agreements and be in accordance with the agreed principles and vision of the Welborne Streets Manual.

Codebreakers

Departures from the Welborne Streets Manual will only be acceptable when a clear justification can be provided and if the deviation can be clearly demonstrated as a positive intervention that has place-making benefits, or responds appropriately to changing legislation and guidance, unforeseen issues, circumstances and/ or technological advancement.

Codebreakers must be an enhancement which are to be agreed with the Town Architect and approved by the Master Developer and FBC. HCC shall also be consulted.

Monitoring & Review

The build out of Welborne will take many years. Over that period, technologies and lifestyles will inevitably change and it is anticipated that aspects of the design codes may need to be adjusted to reflect this. It is important, therefore, that regular monitoring of the design codes takes place to enable lessons learned during preceding phases to inform the design and construction of subsequent phases.

It is anticipated that the design codes will be subject to five- year review and update processes in consultation with Fareham Borough Council and Hampshire County Council. The review process will ensure that Welborne can adapt to changing needs whilst ensuring that the fundamentals of the core vision remain constant.

2b. HOW TO USE THE CODE


The Welborne Streets Manual includes:

Supporting design guidance: content that provides background, explanation and examples to assist with the understanding of the vision that underpins the Welborne.

6c. STREET LIGHTING


At Welborne, street trees are integral to the proposed landscape structure of the plan and so the coordination of lighting columns and trees is a vital aspect of the design.

The lighting must achieve the technical and performance requirements of HCC and be successfully integrated into the proposed tree-lined streets across Welborne.

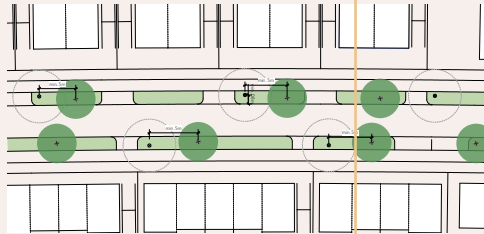


Typical HCC street light

KEY COMPONENTS *(Must be adhered to)*

- Street lighting columns will be located at least 5m from the trunk of trees.
- Street lighting columns to be a minimum of 0.8m set back from the carriageway.
- All street lighting columns will be HCC standard BS48 #1282 green column except for lay areas where there may be back to match street furniture (example Village Centre and District Centre)
- The lighting columns proposed are to be 6m high on primary streets and 5m high on secondary, tertiary and edge lanes. For green links and shared routes, 4m columns or bollard lighting will be proposed.
- Wall mounted lighting on buildings is acceptable in limited instances providing an easement for access to undertake repairs is provided. All junction boxes must be located on the external façade of the building, not in cavities.
- Unadopted roads and footways do not have to be lit to adoptable standards.
- Cycleways and footpaths through open space will be unlit, with the exception of key routes determined on a case by case basis - the route from  to Kiln road across Fareham Common shall be lit.


Right: Illustrative plan showing placement of street lighting relative to street trees



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6d. DRAINAGE

Surface water drainage is required to convey rainwater that falls onto roofs or hard paved areas away from habitable areas, and to ensure that flooding does not occur. At Welborne, the philosophy of surface water drainage is to achieve sustainable, environmentally enriching systems that closely mimic the process seen on a natural landscape. This will include swales within verges, bioretention and permeable paving to capture first flush pollution and minimise run-off from more frequent rainfall events.

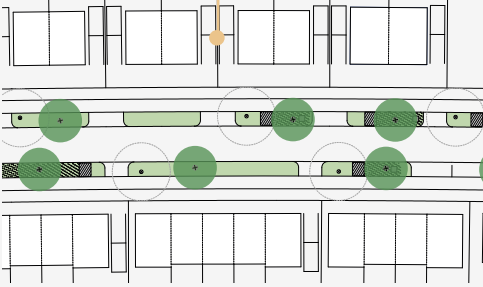


Example of a swale incorporated into a verge

KEY COMPONENTS *(Must be adhered to)*

- Verges may be designed to incorporate surface water swales that will be managed by WGV. These unadopted swales will accept highway drainage.
- Parking bays and driveway crossovers will use permeable paving.
- On primary and secondary roads, highway surface run off, will be managed by WGV as soon as it leaves the extent of the highway.
- Any highway drainage apparatus, adopted by HCC required outside of the adopted highway, will require a maintenance easement.
- All unadopted lay-bys will fall away from the adopted highway.
- All streets and parking courts will incorporate 'interception storage' whereby the first 5mm of rainfall is captured close to source and disposed of via soakage or evaporation.

Right: Illustrative plan showing swales incorporated into verges on a typical street



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Key component design fixes: elements that must be adhered to. Key component design fixes are listed on selected pages with highlighted boxes.



3. PLACEMAKING PRINCIPLES

Welborne will be a mixed-use, mixed-tenure settlement that brings the amenities of small-town life into a verdant setting shaped by high standards of urban and landscape design. Schools, shops, employment centres, local services and parks will be connected via a network of green links to a variety of housing types, enabling a healthy, sociable and walkable lifestyle. The layout and design of Welborne's streets, homes, open space and landscapes, and the interfaces between each of these elements, are shaped by a series of placemaking principles. This section sets out the placemaking principles that are relevant to street design.

A legible place with an interconnected and permeable street network

Welborne will have a clear and easily understood urban structure, providing residents and visitors with a choice of direct, convenient and safe walking and cycling routes that connect homes to shops, parks, public transport routes, schools and other key facilities.

Streets that are well designed and appropriately proportioned

Welborne's streets will be appropriately designed to combine a number of key functions, including safe access and movement, parking and servicing and space for infrastructure and green infrastructure to manage surface water. Streetscapes will be an attractive environment that create amenity and identity for the new settlement's constituent neighbourhoods.



Dashwood Avenue

Streets and public spaces that are safe and well overlooked

Streets will provide high levels of natural surveillance. Inactive frontage and blank façades will generally be avoided. Particular care will be taken in the design of streets to ensure public space is well overlooked.



Welborne Park

Integrated green networks

The hedge- and tree-lined streets form part of an extensive green network, also comprising private gardens, open space and green routes. This network works in conjunction to provide cooling and climate change adaptation solutions, manage surface water and increase opportunities for biodiversity.



Multi-functional green networks

Sufficient and convenient provision of residential car parking

The arrangements for residential car parking will be safe and convenient, whilst at the same time not undermining the quality and walkability of Welborne's streets. A combination of on-plot, on-street and courtyard parking will be provided in ways that are appropriate to context and designed to minimise the visual dominance of the car and hard-surfaced areas.

Parking provision will be in accordance with FBC standards.



Example of parking solutions

4. ADOPTION, MANAGEMENT & MAINTENANCE

4a. WELBORNE GARDEN VILLAGE TRUST

It is a priority to ensure that not only is Welborne built to a high standard, but that this quality is safeguarded in perpetuity and is accompanied by a strong village-life ethos. To this end, Welborne Garden Village Trust (WGVT) will be established, a not-for-profit limited company, as a mechanism for ensuring the long-term stewardship of Welborne for the benefit of its residents.

WGVT will be set up prior to first occupation with board membership offered to representatives from Welborne Land, the Master Developer, the council and the county council.

All unadopted areas of the development will be transferred to the WGVT as a freehold or 999 year lease. This includes play areas, public open space, pedestrian and cycle routes as well as visitors parking bays, trees and verges. This estate management company will be responsible for the management and maintenance of these areas in perpetuity.

WGVT will also be responsible for the planned inspections, day to day maintenance, insurances and parking enforcement of all unadopted areas.



WGVT will carry the responsibility for ensuring that non-adopted areas of the streetscape are maintained

4b. ADOPTION

In general, where roads meet the technical standards required by Hampshire County Council (HCC), serving 10 dwellings or more they will be adopted. Agreement is required from HCC for unadopted roads serving between 10-50 dwellings where adoption is not considered to be in the public interest.

The extent of the adopted highway will be agreed on a case-by-case basis through the S38 design audit process and finalised through the applicable S38 agreement. As a minimum, the extent of the

adopted highway shall be from back of kerb or channel to the opposite back of kerb or channel. This shall be the default extent of adoption unless there is a specific strategic need, as designated by the Highway Authority, for adoption of additional areas and assets. All verges, parking bays, swales, cycleways and footways shall remain unadopted and be maintained in perpetuity by the WGVT. In the event WGVT fail to perform any of their obligations, HCC are entitled to access land to carry out necessary works and recover the costs incurred from doing so from WGVT.



4c. HIGHWAY ASSETS IN EASEMENT

STREET LIGHTING

All street lighting (including any illuminated street furniture and feeder pillars if and where applicable), will be adopted under the HCC PFI contract, subject to them meeting technical requirements. All columns and lit features to be set back as per TG 13 (minimum of 0.8m from carriageway). No multi directional LED's will be accepted. HCC will adopt wall mounted lighting on buildings with an easement for access to undertake repairs, if an alternative solution cannot be found. All junction boxes etc must be located on the external façade of the building, not in cavities.

All street lighting columns will be HCC standard BS48 #12B21 green column except for key areas where there may be black to match street furniture (example Village Centre), which will incur a Commuted Sum.

SIGNS

Any road signage required for the adoption of the road, will be managed, and maintained by HCC. All street name signage will be maintained by WGVT and outside of dedicated highway.

DRAINAGE

Any highway drainage apparatus, (drainage only taking highways surface run off, which HCC are to maintain in perpetuity), required outside of the adopted highway, will require a maintenance easement including a 3m offset for maintenance purposes, if not within the wider default easement area outlined above.

Verges may be designed to incorporate surface water swales that will be managed by WGVT. These unadopted swales will accept highway drainage.

On primary and secondary roads, highway surface run off, will be managed by WGVT as soon as it leaves the extent of the highway.

NOTE: All sewers under the adopted highway are to be adopted by a water authority or NAV company. All other pipework (for example connecting swales to the sewers) shall be adopted and maintained by the WGVT.



Street lighting columns to be in HCC standard colour



Verges may be designed to incorporate water swales

4d. HIGHWAY EASEMENTS & RESTRICTIVE COVENANTS

As HCC will have a reduced area of adopted highway than usually provided, an easement will be required to ensure that the County Council can undertake its statutory function as local highway authority.

HIGHWAY EASEMENT

The Highways Easement area will be defined on a case-by-case basis through the S38 design audit process and finalised through the S38 agreement. By default, on primary and secondary streets, the easement shall extend from the back of kerb or channel to the back edge of footway and include any intervening verge, swale, parking bay, cycleway or footway. The minimum extent of easement, on tertiary roads, shall be a 0.5m strip from back face of kerb or channel. Any other highways assets outside of this default area will also be required to be covered by an easement.

The Easement will grant HCC the following free and unfettered rights and will need to be entered into upon establishment of highway rights as part of the S38 agreements:

- To enter upon the easement land as and when required, with or without vehicles, plant, machinery, contractors for the purpose of inspection, maintenance, repair or renewal of any equipment, assets or works associated with maintaining the highway
- The right to erect any necessary temporary fencing, signage, traffic management equipment or barriers on the easement land
- The right to remove any trees, hedges, shrubs or other structures built which restrict the Council's ability in maintaining and keeping safe the highway or any of their assets within the easement land
- The right to install new assets, such as drainage, signage, street lighting, lit street furniture, ITS and all other auxiliary equipment required for safety and improvements schemes, following notification of intent to WGVT

HCC under the easement will act reasonably and reinstate to the condition prior to entry onto the easement land and make good, as soon as reasonably practicable, any such damage caused to the land.

RESTRICTIVE COVENANT

All junction visibility splays, inter-visibility and forward visibility required for the adopted roads and where pedestrian/cyclist visibility at crossing locations is required, will be covered by a restrictive covenant, where there isn't a strategic requirement for them to be adopted. WGVT will be bound to the restrictive covenant and be required to:

- Keep the land in good repair and condition
- Maintain, repair, replace, renew and keep any private retaining walls bordering the land in good state of repair and condition
- Not to do or allow anything on the land which could compromise the adjoining highway
- Not to do or allow any building, wall, fence, structure or trees, shrubs, hedges or soft landscaping to exceed 600mm of height
- Not to use the land for anything other than a visibility splay for the benefit of the adjoining highway
- To indemnify the County Council in respect of any liabilities, actions, costs, charges, claims, demands and expenses incurred by the County Council arising out of or incidental to or in connection with WGVT's obligations under the above terms
- In the event WGVT fail to perform any of their obligations, HCC are entitled to access the restrictive covenant land to carry out necessary works and recover the costs incurred from doing so from WGVT
- Comply with all other usual reasonable requirements required by HCC pertaining to restrictive covenants being registered and required where land is disposed/transferred.

4e. MAINTENANCE

WGVT will be responsible for maintaining unadopted areas, including but not limited to, the following:

- Winter maintenance as per HCC guidelines
- Management and maintenance of trees for safety and actionable nuisance and soft landscaping to avoid impact on street lighting and visibility splays
- Management and maintenance of pedestrian and cycle routes, including parking bays
- Maintaining unrestricted public access to all footpaths and cycleways within the highway easement boundary
- Grant the right to the uninterrupted free passage and discharge of surface water run off, at all times, in perpetuity
- Not build any structure or obstruct or impede the discharge water runoff from the adjoining highway
- Retain, inspect, adjust, repair, alter and fully maintain any drainage features taking highway surface water runoff in perpetuity
- Maintain works in such good repair and condition to accept storm water and surface water from the highway so that it does not cause damage to the highway
- To indemnify the County Council in respect of any liabilities, actions, costs, charges, claims, demands and expenses incurred by the County Council arising out of or incidental to or in connection with WGVT's obligations under the above terms
- Grant the right to HCC to make future connections without fee or charge
- Grant the right to enter land without notice, for the purpose of inspection repair renewal and maintenance of the WGVT maintained drainage network, in the event of a breach of the obligations above and recover in full from WGVT all expenses and costs it incurs in so doing
- Comply with all other usual reasonable requirements requested by HCC pertaining to the requirements being registered against the title and required where land is disposed/ transferred.

The management and maintenance will be in line with HCC maintenance regime for public highways as set out in Highway Safety Inspection Manual version 1.7, 18 October 2021 (or any subsequent superseding guidance).

AREAS ADJACENT TO HIGHWAY

A covenant will be placed on all residents to maintain their property frontage. The WGVT will secure step in rights with the ability to undertake maintenance works to areas directly affecting the public highway (e.g., trimming of hedgerows and overhanging trees).

Permitted development rights will be removed for the replacement of any permeable driveways and front boundaries. Any alterations or resurfacing will require WGVT's approval as well as planning approval.



WGVT will secure step rights with the ability to undertake maintenance works to items such as hedging

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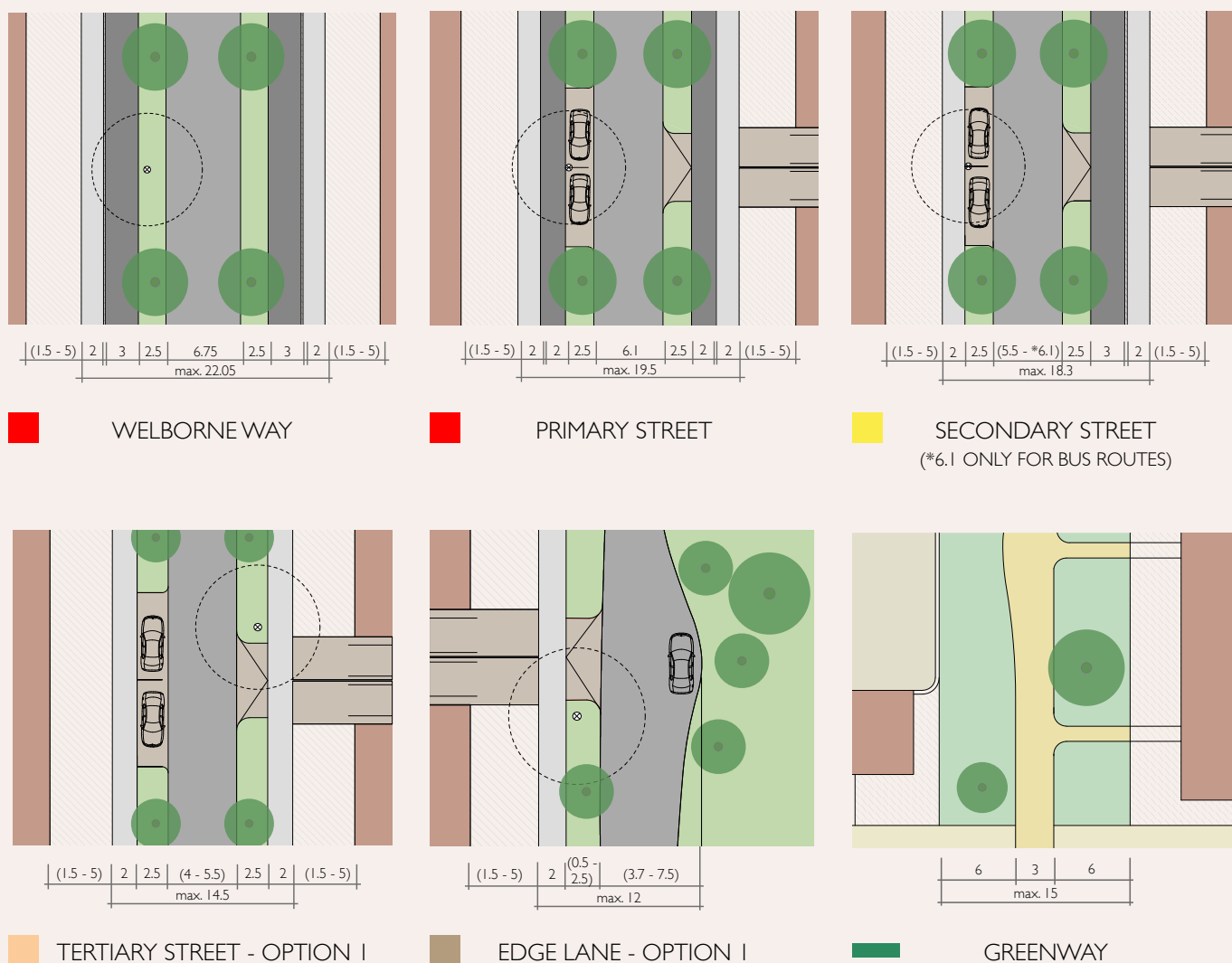
5. MOVEMENT STRATEGY

5a. STREET HIERARCHY

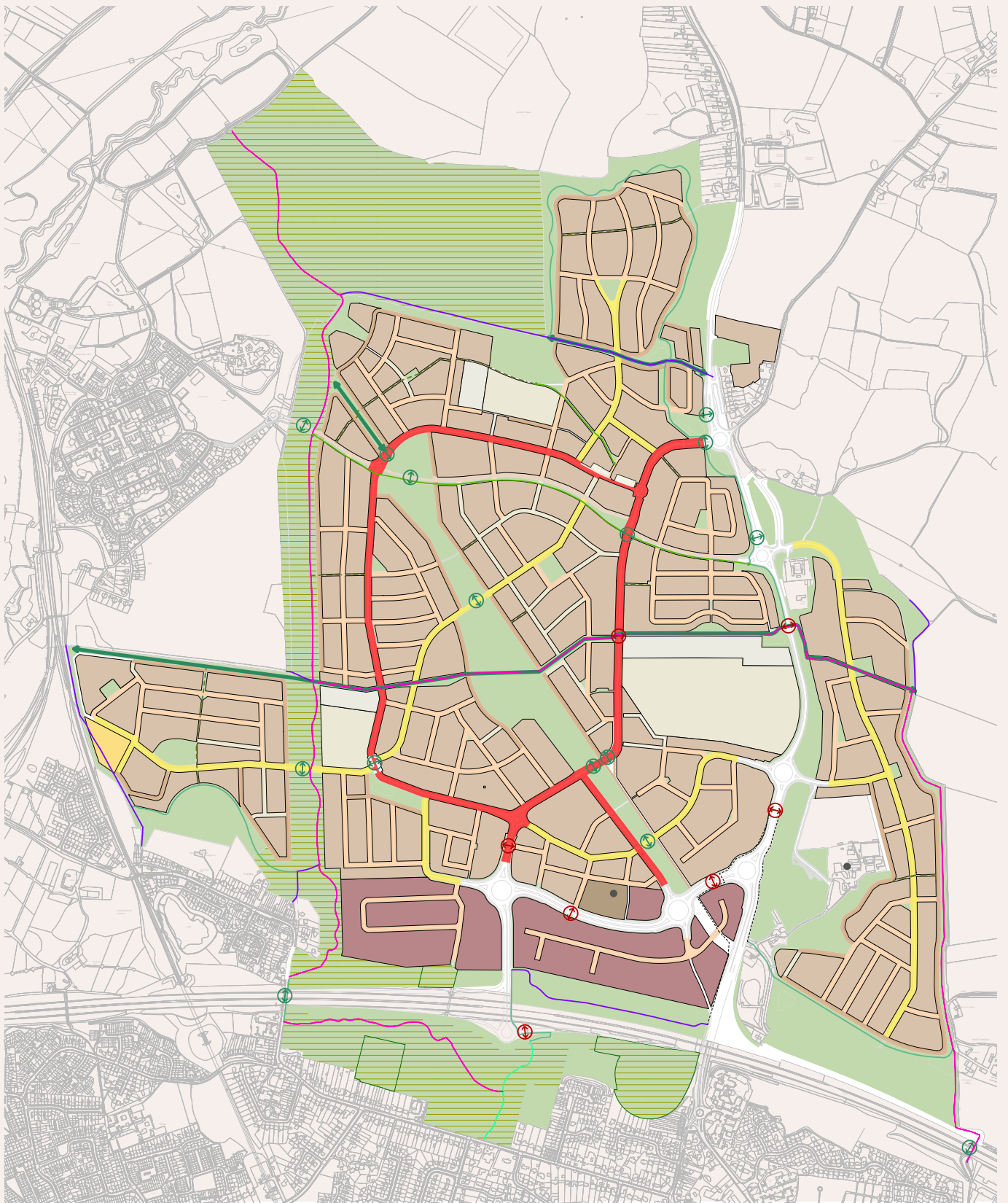
A defining element of Welborne will be its clear hierarchy of street types based on connectivity and importance of route. The hierarchy of streets is based on a carefully considered rationale that focuses on safety and legibility for all use.

Walking and cycling are intended to be the main methods of transportation. Safe cycling routes are well integrated in the street network through the provision of cycleways on Welborne Way, primary and secondary streets. Junctions and crossroads are equally a key feature; at Welborne they will be designed to prioritise pedestrian and cycle movement.

A selection of the street types that form the street network at Welborne, further detail is provided in Section 7.



ILLUSTRATIVE STREET HIERARCHY PLAN



KEY

- Primary street
- Secondary street
- Tertiary street
- Edge lane
- Knowle Road
- Tertiary street (Science and Technology Park)
- Rail halt (if required)
- PRoW brideway
3m-wide where new
- PRoW footpath
1.5m-wide where new
- Unsignalised pedestrian crossing
- Signalised pedestrian crossing
- Greenways incorporating PRoW brideways and footpaths as indicated
- Strategic green link
- Illustrative green link
- Strategic open space route
- Illustrative open space route
- 10K loop connection

5b. JUNCTIONS & CROSSROADS

At Welborne junctions and crossroads are an opportunity to assist with the placemaking principles and vision. Junctions and crossroads will be designed to give priority to pedestrians and cyclists to enable them to move around easily and safely. On primary and secondary streets crossings will be provided on a raised surface, so that pedestrians and cyclists can cross on a level surface. This will act to slow traffic on the approach to the crossing as part of a slow speed streets strategy.

Knowle Road will also include raised surface crossing junctions.

Selected key junctions will also require a special enhanced design response, these are where important junctions occur in locations such as the Village Centre and District Centre.

Details on key junctions, crossroads and typical junctions are provided in section 7.

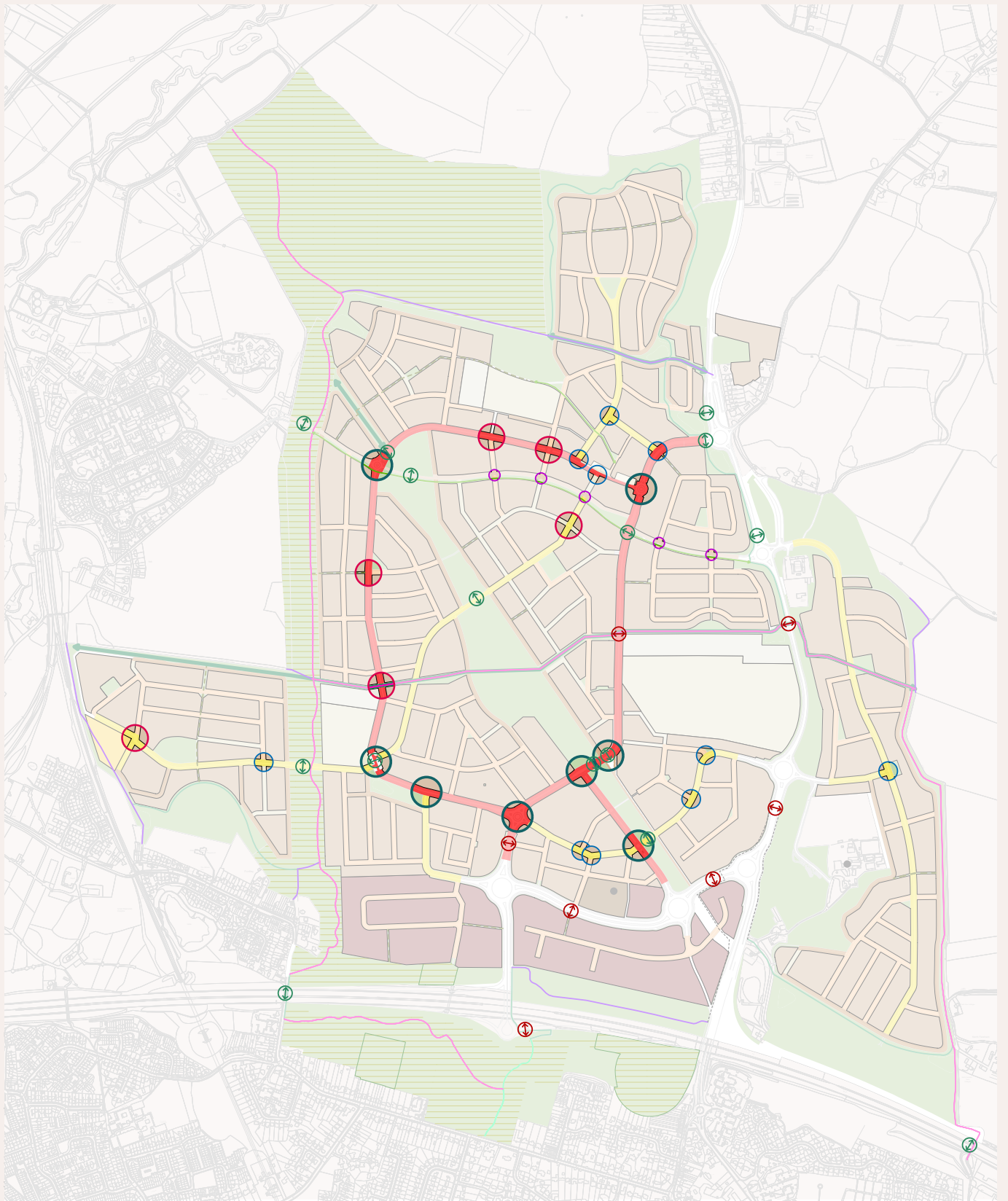
Example of a crossroad on a primary street









KEY

- | | | |
|--|---|--|
| <ul style="list-style-type: none"> Footway (Paving Slabs) Carriageway (Tarmac) Cycleway (Tarmac) Tree verge Concrete Kerbs (100mm high)
<small>Conservation kerbs in special places</small> Tactile Paving Granite slabs | <ul style="list-style-type: none"> Street lamp (min. 5m offset to tree trunk) Trees Parking & Driveway Crossovers (Permeable block paving) Crossroad Junction Surface (Resin bonded gravel) Cycleway Tactile Paving Integrated Cycleway | <ul style="list-style-type: none"> Granite Setts Grip Catchpit/Gully Bioretention System Dropped flush kerb for drainage Adoption Boundary |
|--|---|--|

KEY JUNCTIONS PLAN



KEY

-  Key Enhanced Junctions on Primary Street
 -  Crossroads / Pedestrian Crossovers on Primary and Secondary Streets
 -  T - Junctions on Primary and Secondary Streets
-  Knowle Road Crossroads / Pedestrian Crossovers
 -  Unsignalled Crossing Point
 -  Signalled Crossing Point

5c. ACTIVE TRAVEL STRATEGY

A proposed hierarchy of active travel routes within the street network and open spaces make best use of existing footways and cycling provision and connect the community to local facilities. This will enable walking and cycling for everyday activities, such as getting to school or to the shops, as well as recreational loops and connections out to the wider countryside and into Fareham.

Key infrastructure improvements will be provided on Kiln Road, Maylings Farm Road, Miller Drive, Arundel Drive, Grove Road, Park Lane, Westbury Path, Funtley Road, A32 Wickham Road, Broadcut and Highlands Road. These facilities, together with improvements provided through Fareham Leisure Centre, will create sustainable transport links, promoting active travel to Fareham town centre, railway station, educational and leisure facilities. Improvements to the A32 Hoads Hill, A334 Winchester Road together with Lavey's Lane/Fontley Road will provide links for active travel towards Wickham and Whiteley.

WALKING

The Welborne masterplan builds on the 'walkable neighbourhood' concept, where a local centre with facilities are within a 5- to 10-minute (approximately 400m) walking distance.

CYCLING

The network of cycling routes aims to serve a variety of different users. These will be distributed throughout the network. Some will be dedicated to cyclists, some will be shared. Likewise, some routes will be on streets and some will be on specific green routes. Appropriately located and designed cycle parking is to be provided at local community facilities in accordance with Fareham's cycle parking standards. Provision should be conveniently located, easy to use and secure.

HORSE RIDING

Sections of the existing PRow network or diverted routes will be upgraded to provide a continuous bridleway link from Fareham. Horse box parking facilities will be provided at Funtley Hill and Dashwood SANG carpark.

KEY COMPONENTS *(Must be adhered to)*

Paths within the street network:

- **Welborne Way:** There will be dedicated 3m cycleways on both sides with segregated footways.
- **Primary streets:** There will be dedicated 2m one way cycleways on both sides with segregated footways.
- **Secondary streets:** There will be a dedicated 3m two way cycleway on one side with segregated footways.
- **Tertiary streets and edge lanes:** Varies per option, pedestrian footway on one side or no footway. These streets and lanes will be designed with an emphasis on pedestrians and cyclist comfort so cycling along the carriageway feels safe.
- **Safe routes to schools:** All primary and secondary streets shall be designated with safe routes to schools.

Right: Illustrative image of a proposed active travel route



Paths within open space:

The design of development blocks should ensure that they provide natural surveillance onto green spaces effectively and there is easy access to green corridors for recreational purposes.

- Provision has been made for recreational routes that create a village circuit as part of the 10k Park around the whole Garden Village and smaller circuits of varying lengths around neighbourhoods. These multifunctional routes will provide the development with nature and fitness trails at appropriate locations and with a legible wayfinding strategy. Recreational loops are provided as part of the three SANGs. See Strategic Design Code for further detail.

PUBLIC RIGHTS OFWAY STRATEGY

The site benefits from good access to existing Public Rights ofWay within the site and connecting beyond to Fareham, Funtley, Knowle, Wickham and the River Meon.

The strategy for retaining, diverting, upgrading, or stopping up existing Public Rights ofWay is summarised in the table below and illustrated on the plan opposite.

Table 1:










Strategy for retaining, diverting, upgrading or stopping up existing Public Rights ofWay, subject to statutory approval

Footpath 15	Existing PRow to be retained - outside of site boundary
Footpath 23a	Existing PRow Footpath 23a to be upgraded to a bridleway, to provide a multi-user connection to Mayles Lane.
Footpath 23b	Eastern section to be retained in current form; western section to be diverted
Footpath 85	To be resurfaced. Possible part-diversion of PRow, subject to potential rail halt detail design
Footpath 86	Existing PRow to be diverted and extended to link to Funtley Hill and upgraded to bridleway status
Footpath 87	Existing PRow to be retained, with part diversion to the south at the easternmost extent, as to provide a connection with the proposed informal pedestrian crossing in this location
Footpath 88	Existing PRow may be extinguished and replaced by a new east-west multi-user green link to the north
Footpath 89	Potential part diversion of the western half. Diversion to go south of the M27 into Fareham Common
Footpath 90	Northern half stopped up and subway closed
Bridleway 100	Existing PRow to be retained and upgraded
Footpath 101	Existing PRow footpath to be diverted onto site, upgraded to a bridleway and extended to the south to provide a connection to the existing bridleway 100 and M27 bridge
Footpath 102	Existing PRow retained as part of new east-west multi-user PRow to the north, through new green link. Diversion to accommodate development and link to new pegasus crossing
Footpath 103	Existing PRow to be part-diverted to the east to align with proposed site boundary

PUBLIC RIGHTS OF WAY PLAN



KEY

- | | | | | |
|---|--|--|--|--|
| Public Rights of Way | |  Proposed diversion/upgrade to
bridleway | Existing Cycle Routes | |
|  Existing PRoW retained |  Multi-user link from M27 J10 works
to Kiln Road |  Shared-use paths retained |  Shared-use paths realigned | |
|  Existing PRoW stopped up/diverted | National Cycle Network |  Proposed shared-used path route | | |
|  Proposed diversion/upgrade to
footpath |  NCN 224 | | | |

5d. PUBLIC TRANSPORT STRATEGY

Welborne will provide a new bus route to serve the development. The proposed route will link the development site to Fareham railway station, bus station and the town centre.

The proposed route will enter Welborne via a new bus-only link located to the south of the Broadway roundabout. This would pass through the site via the High Street roundabout, connecting to the wider site via a bus-only link through the District Centre. It would then complete a circular course along the primary routes through the site including the western loop (West Way, North Drive) as well as serving the Village and District Centres and the Community Hub via Dashwood and Welborne Way.

Buses would leave the site via the Broadway roundabout, having returned to the District Centre via the bus-only link from Welborne Way.

Subject to operator requirements, the proposed bus service could operate as an extension to the existing Eclipse BRT service, or as its own independent route. Generally, bus stops will be provided within the site as illustrated on the plan opposite. The precise routing and location of bus stops will be subject to agreements with the bus operators and dealt with at Reserved Matters stage.

Welborne will also be served by the existing Route 69 that currently operates between Fareham and Winchester. There is also an existing bus Route 20 which runs between Wickham and Fareham.






Bus priority measures are provided on external roads to improve service reliability and journey times, with dedicated bus lanes available along the A32 between the Broadway roundabout and North Hill.

PUBLIC TRANSPORT PLAN: ROUTES AND STOPS








KEY

Existing public transport

-  Retained Fareham to Wickham (route 20)
-  Retained Winchester to Fareham (route 69)
-  Retained bus stop
-  Relocated existing bus stop
-  New bus stop on Knowle Road (route 20)

Proposed public transport

-  Welborne and Fareham Town one-way service on carriageway and indicative bus stops
-  Dedicated bus lane section and indicative bus stops

-  Rail halt (if required)
-  Village Centre and District Centre
-  Community facility

6. STREET DESIGN PRINCIPLES

6a. TYPICAL STREET & JUNCTION DESIGN

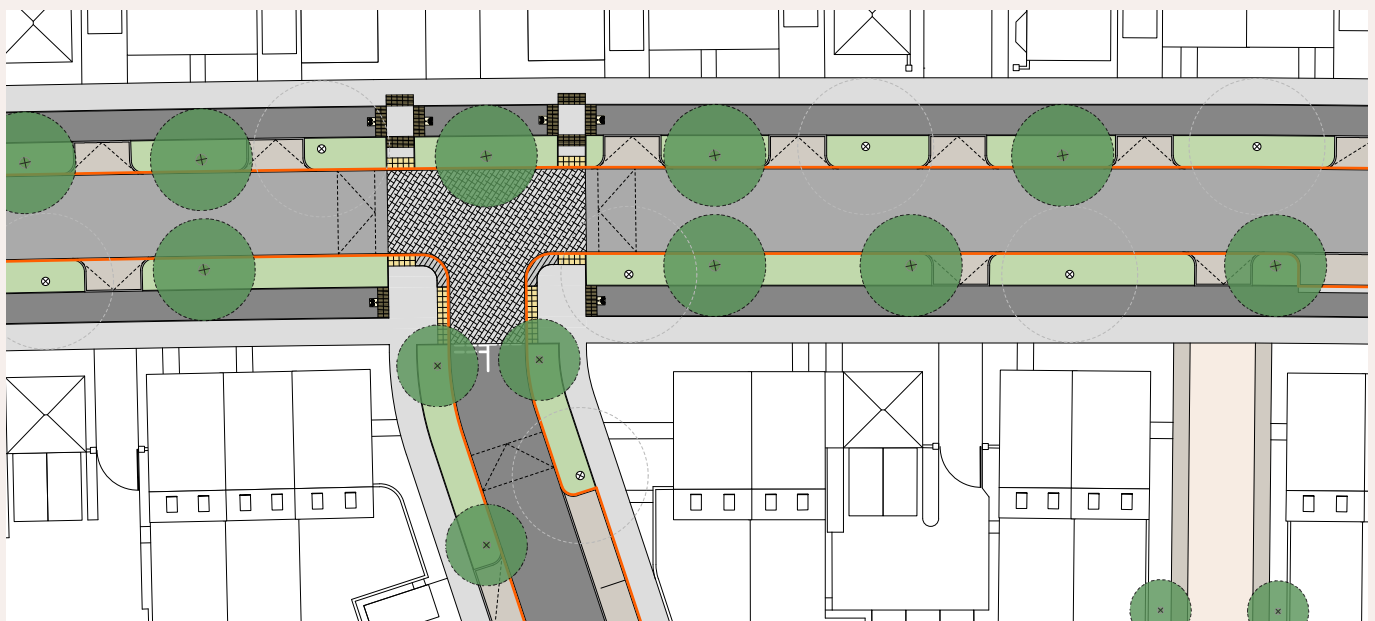
This section considers how the streets will look and feel for people moving around Welborne. The design of the streets will be influenced by several overarching design principles alongside technical requirements.

KEY COMPONENTS *(Must be adhered to)*

- 2.5m wide verges shall be incorporated on all Primary, Secondary, Tertiary and Edge Lane 1 and 2 street types.
- 3m cycleways on both sides of the street will be incorporated on Welborne Way
- 2m one way cycleways on both sides of the street will be incorporated on primary streets.
- A 3m two way cycleway will be incorporated on one side of a secondary street.
- Junctions on primary and secondary streets will be designed to give priority to pedestrians and cyclists.

- Where raised surface junctions are utilised the vehicular approach to these will be a 1.37 gradient over 3 kerb lengths.
- Raised surfaces that are to be adopted will use a resin bonded surface finish on primary roads and bus routes, block paving is acceptable elsewhere.
- Driveway crossovers and in verge parking spaces will be surfaced in permeable block paving and use a 1m radius quadrant kerb detail
- Street trees will be incorporated in all verges, with street lighting columns located at least 5m from the trunk of trees.
- The design detail of verges will discourage parking on the verge by providing a 100mm kerb, the introduction of planting and trees and where required bollards.

Primary Street incorporating a raised surface junction



KEY

- | | | |
|---|--|---------------------------------|
| Footway (Paving Slabs) | Street lamp (min. 5m offset to tree trunk) | Granite Sett Grip |
| Carriageway (Tarmac) | Trees | Catchpit/Gully |
| Cycleway (Tarmac) | Parking & Driveway Crossovers (Permeable block paving) | Bioretention System |
| Tree verge | Crossroad Junction Surface (Resin bounded gravel) | Dropped flush kerb for drainage |
| Concrete Kerbs (100mm high)
Conservation kerbs in special places | Cycleway Tactile Paving | Adoption Boundary |
| Tactile Paving | Integrated Cycleway | |

6b. STREET TREES

At the heart of the vision for Welborne is to create streets lined with trees and hedges. The streets will be designed to accommodate trees with mature canopy spread supported by management of the trees by WGVT over their lifetime.

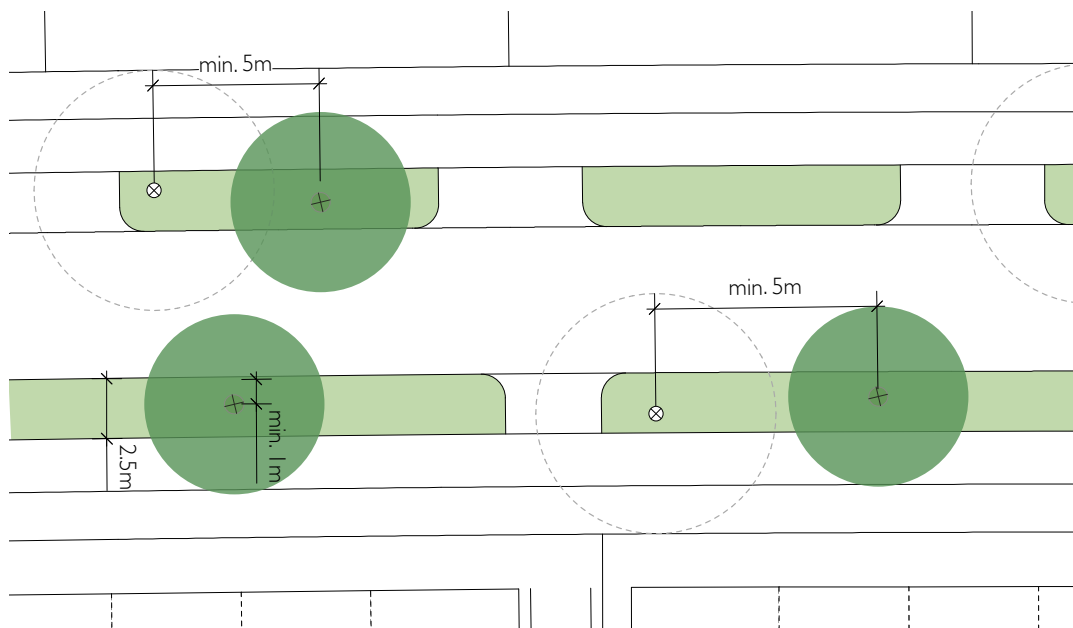
Siting of street trees from the public highway will depend on visibility splays, driveway cross overs and street light column spacing. Also to be considered in the detailed design shall be the chosen tree species, required soil volumes for the tree and provision of root protection barriers. Soil volume for street trees will include trenching under verge driveway crossovers and verge visitor parking bars.



Streets designed to incorporate verges, trees and hedges to property boundaries

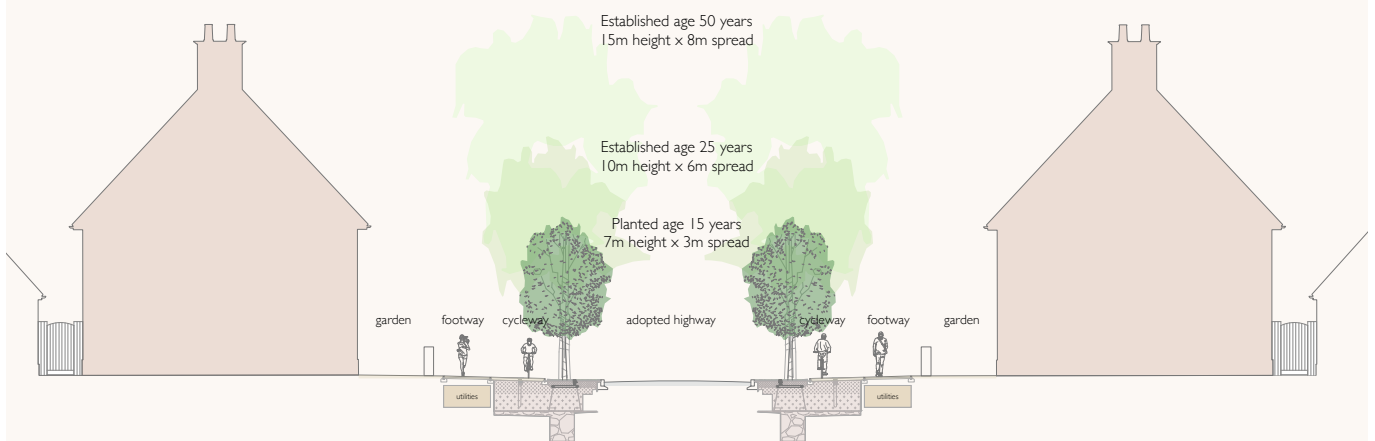
KEY COMPONENTS *(Must be adhered to)*

- Verge widths on streets with trees will typically be 2.5m.
- Street trees will be centred a minimum of 1m from the edge of the carriageway. Dimension taken from the centre of the tree trunk.
- On occasion trees may be sited within junction visibility splays to achieve the Welborne design vision of tree lined streets. *Due to the low design speeds throughout the development, isolated obstructions should not pose an unacceptable safety risk. The principle is supported by Manual for Streets, which states 'occasional obstacles to visibility that are not large enough to fully obscure a whole vehicle or a pedestrian, including a child or wheelchair user, will not have a significant impact on road safety' (DfT, 2007, p. 94).* Trees may also be sited in visibility splays for front driveway access.
- Street lighting columns will be located at least 5m from the trunk of trees.
- There will be a 1m minimum gap between garden trees and the highway's adopted edge.



The following typical sections demonstrate how street trees (with the required soil volume) and drainage will be incorporated on a primary, secondary and tertiary street..

Primary Street cross-section



Primary Street Trees

Where verges extend alongside cycleways

Large Tree Species provided with 36m³ rooting volume of Stockholm System (equivalent to 28m³ open soft ground)

Supply size 35 - 45cm girth

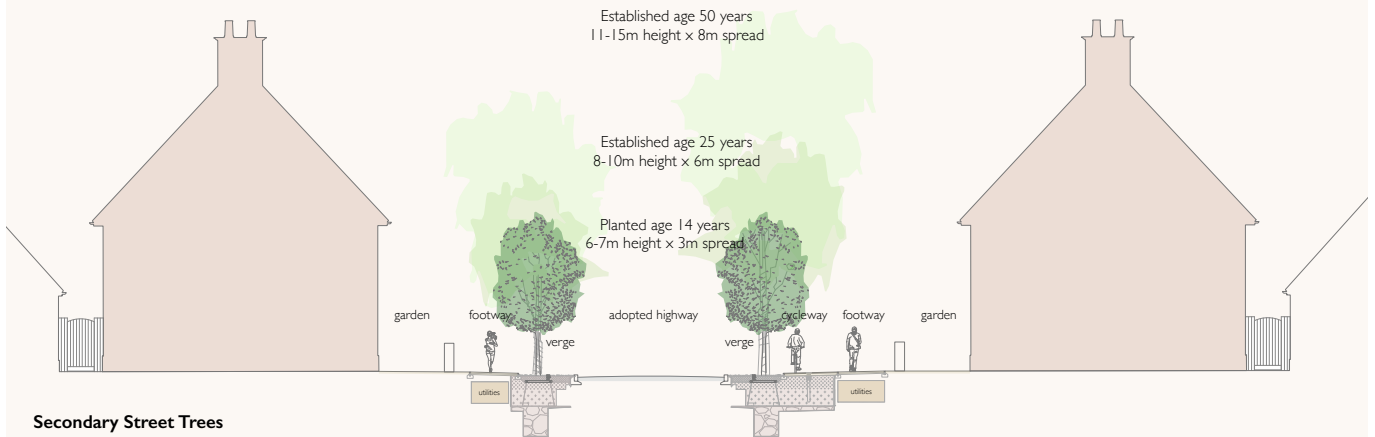
4x transplanted (circa 15 years age)

Tilia cordata 'Greenspire'

Illustrative image of a Primary Street



Secondary Street cross-section



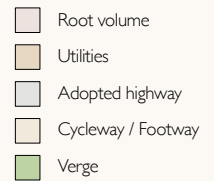
Secondary Street Trees

Where verges extend alongside footways & cycleways

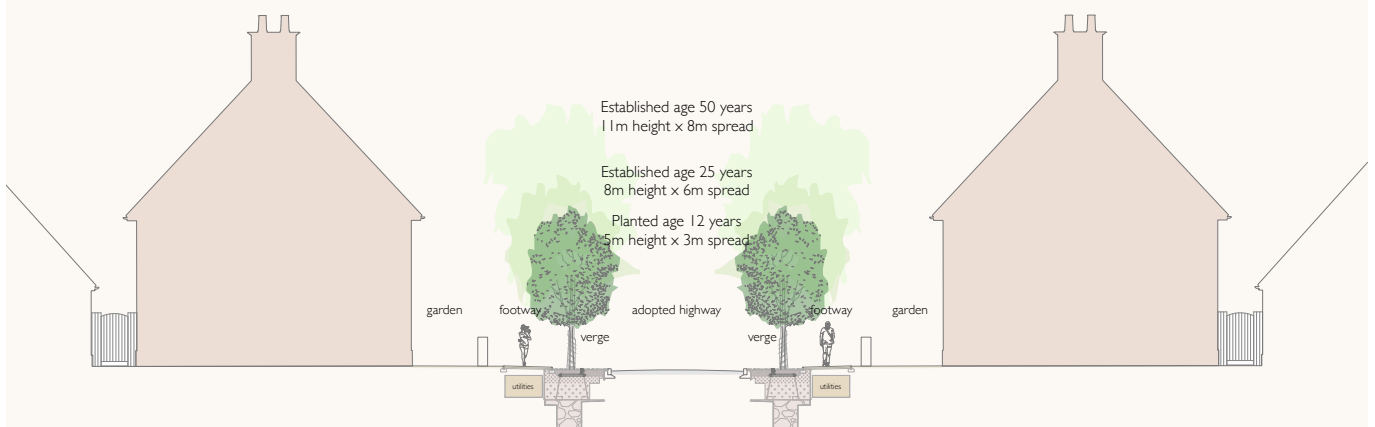
Large Tree Species provided with 36m³ rooting volume of Stockholm System (equivalent to 28m³ open soft ground)
Medium Tree Species provided with 26m³ rooting volume of Stockholm System (equivalent to 20m³ open soft ground)

Supply size 30 - 40cm girth
4x transplanted (circa 14 years age)

Carpinus betulus (Large), Ulmus 'New Horizon' (Medium)



Tertiary Street cross-section



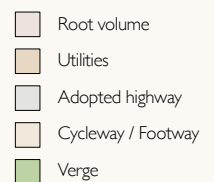
Tertiary Street Trees

Where verges extend alongside footways

Medium Tree Species provided with 26m³ rooting volume of Stockholm System (equivalent to 20m³ open soft ground)

Supply size 20 - 25cm girth
4x transplanted (circa 12 years age)

Acer campestre



6c. STREET LIGHTING

At Welborne, street trees are integral to the proposed landscape structure of the plan and so the coordination of lighting columns and trees is a vital aspect of the design.

The lighting must achieve the technical and performance requirements of HCC and be successfully integrated into the proposed tree-lined streets across Welborne.

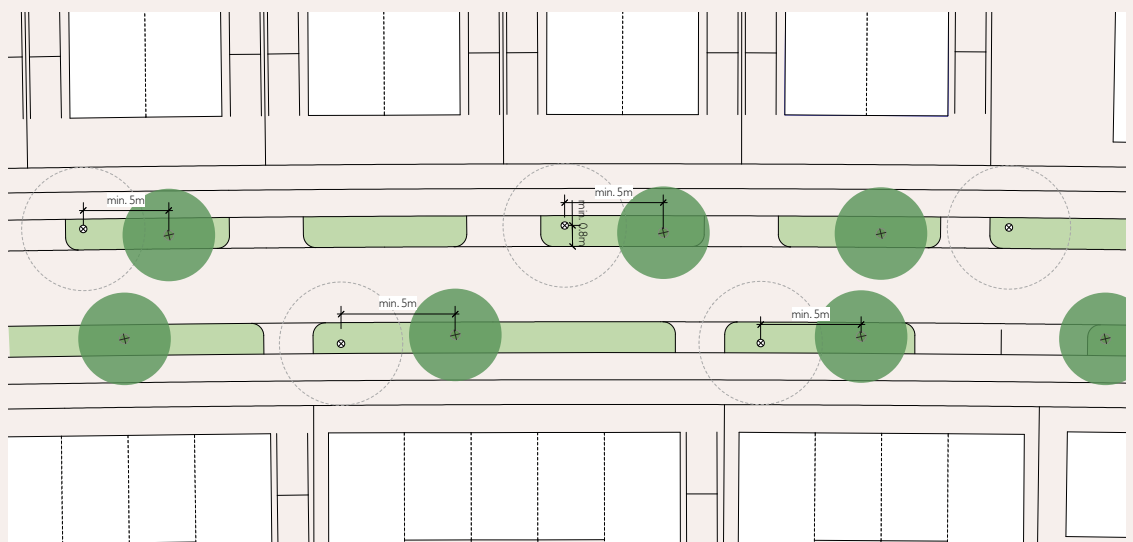


Typical HCC street light

KEY COMPONENTS *(Must be adhered to)*

- Street lighting columns will be located at least 5m from the trunk of trees.
- Street lighting columns to be a minimum of 0.8m set back from the carriageway.
- All street lighting columns will be HCC standard BS48 #12B21 green column except for key areas where there may be black to match street furniture (example Village Centre and District Centre)
- The lighting columns proposed are to be 6m high on primary streets and 5m high on secondary, tertiary and edge lanes. For green links and shared routes, 4m columns or bollard lighting will be proposed.
- Wall mounted lighting on buildings is acceptable in limited instances providing an easement for access to undertake repairs is provided. All junction boxes must be located on the external façade of the building, not in cavities.
- Unadopted roads and footways do not have to be lit to adoptable standards.
- Cycleways and footpaths through open space will be unlit, with the exception of key routes determined on a case by case basis - the route from J10 to Kiln road across Fareham Common shall be lit.

Right: Illustrative plan showing placement of street lighting relative to street trees



6d. DRAINAGE

Surface water drainage is required to convey rainwater that falls onto roofs or hard paved areas away from habitable areas, and to ensure that flooding does not occur. At Welborne, the philosophy of surface water drainage is to achieve sustainable, environmentally enriching systems that closely mimic the process seen on a natural landscape. This will include swales within verges, bioretention and permeable paving to capture first flush pollution and minimise run-off from more frequent rainfall events.

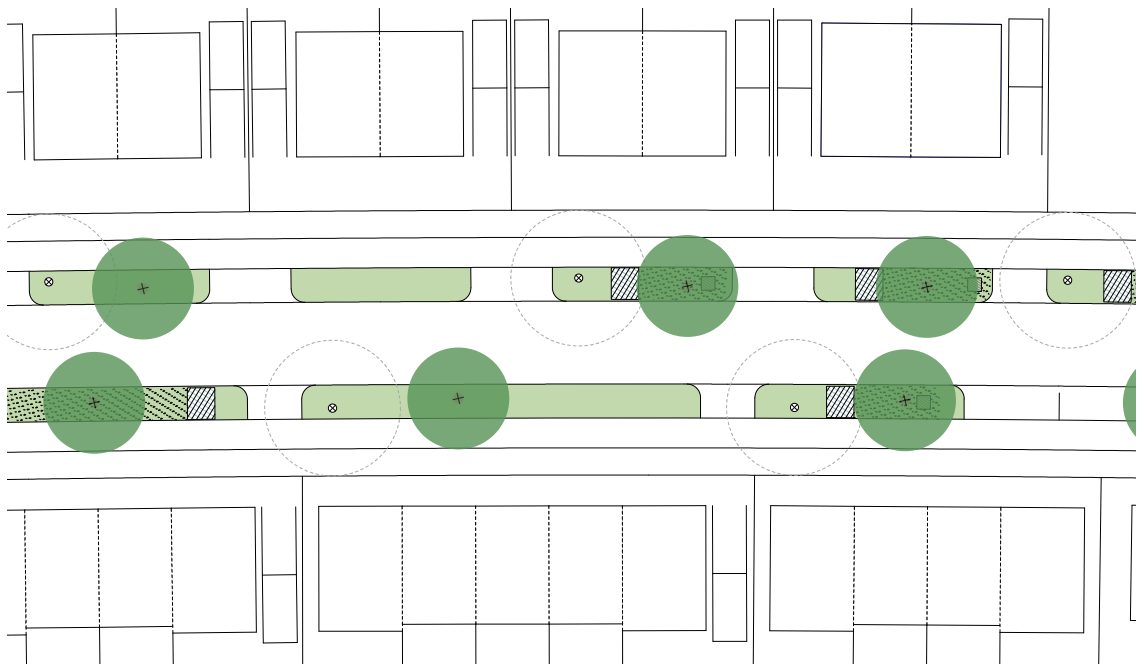


Example of a swale incorporated into a verge

KEY COMPONENTS *(Must be adhered to)*

- Verges may be designed to incorporate surface water swales that will be managed by WGVT. These unadopted swales will accept highway drainage.
- Parking bays and driveway crossovers will use permeable paving.
- On primary and secondary roads, highway surface run off, will be managed by WGVT as soon as it leaves the extent of the highway.
- Any highway drainage apparatus, adopted by HCC required outside of the adopted highway, will require a maintenance easement.
- All unadopted lay-bys will fall away from the adopted highway.
- All streets and parking courts will incorporate 'interception storage' whereby the first 5mm of rainfall is captured close to source and disposed of via soakage or evaporation.

Right: Illustrative plan showing swales incorporated into verges on a typical street



6e. SIGNS & LINES

Signage and lining is to be kept to an absolute minimum at Welborne in order to create a high-quality public realm and great streets without visual clutter. In short, any sign or line will need to earn its place.

KEY COMPONENTS *(Must be adhered to)*

- Only those signs or lines that are deemed absolutely necessary will be permitted. Some examples of lining that will be required include bus cages on Welborne Way, centre lines and give way markings on significant bends or junctions and safety lining associated with schools.
- If highway signs are required, then they should preferably be co-located on lighting columns or placed on buildings.
- To reduce clutter on footways, street naming signs should be placed on adjacent walls or buildings.
- The positioning of signage should not create an obstruction to pedestrian movement, entrance ways or street cleansing, and must not impede highways visibility splays.
- To ensure that the sustainable transport objectives are met, way-marking should be clear, logical and user friendly.
- Bespoke street signs and general town signage will be designed for Welborne in accordance with the design aspirations and intended vision. The design of this signage will be agreed with FBC and KCC.
- All Highway lines and signage will be compliant with Traffic Signs Regulations and General Directions 2016 & TGI2.



Signage and lines shall be kept to a minimum in the street scene at Welborne.



Bespoke street signs and general town signage will be created for Welborne.

6f. MATERIALS & STREET FURNITURE

The overarching principles for the public realm design are that design choices relating to materials and street furniture provide a coherence and legibility across Welborne. Welborne's strategy regarding materials and street furniture promotes simplicity and easy maintenance.

HARD LANDSCAPING MATERIALS

The material palette for the streets within Welborne is divided into three main approaches: typical treatment, informal treatment and special places. This palette will create a varied sense of place where the different treatments are utilised aiding legibility.

Where shared space principles apply, extra consideration should be given to tactile hazard warning paving and contrasting colour palette to aid navigation for everyone and promote an inclusive design approach.

Typical treatment (Hampshire adoptable standards)

- Typical primary, secondary, tertiary streets and courtyard lanes.
- A simple, robust and durable palette of materials for the majority of residential streets.

Informal treatment

- Edge lanes, greenways and green links.
- Informal softer material specification to reflect natural edge and landscape locations.

Special places

- Streets and spaces in relation to urban squares and areas within Welborne Park, neighbourhood parks and community green spaces.
- High-quality materials to highlight a change in the setting from typical residential streets to civic spaces.
- These areas may be considered for adoption and the materials chosen must be in accordance with HCC requirements. Where specified materials do not meet HCC requirements these areas shall be the responsibility of WGV.
- Where shared space principles apply, extra consideration should be given to tactile hazard warning paving and contrasting colour palette to aid navigation for everyone and promote an inclusive design approach.

TYPICAL TREATMENT

ROADS



Hot rolled asphalt



Resin bonded gravel

FOOTWAYS



Concrete paving slabs

CYCLEWAYS



Hot rolled asphalt



Integrated cycleway symbol

KERBS



Concrete



Kerb channel drain to swale system

CHANGE OF SURFACE



Concrete setts in contrasting colour (example use include transition strips)



Granite Setts in contrasting colour (example uses include drainage areas in verges and at and junctions)

PARKING & DRIVEWAYS



Permeable concrete setts



Gravel

Where gravel is used for a driveway it will be set back a minimum of 0.5m from the footway to prevent spilling onto highways. A paved or asphalt strip will be required to separate.

INFORMAL TREATMENT

ROADS



Hot rolled asphalt

FOOTWAYS



Asphalt (above)
Breedon gravel (below)
Surfaced dressed gravel



Concrete paving slabs

CYCLEWAYS



Breedon gravel



Integrated cycleway symbol

KERBS



Concrete



Timber edge (above)
Concrete 50mm kerb (below)



No edge

Gravel-path edging can be omitted and the grass or adjacent vegetation allowed to encroach on the edges of the path for a softer edge and to avoid edging becoming trip hazards for cyclists

CHANGE OF SURFACE



Concrete sets in contrasting colour
(example use include transition strips)



Granite sets in contrasting colour
(example uses include drainage areas in verges and at and junctions)

PARKING & DRIVEWAYS



Gravel

Where gravel is used for a driveway it will be set back a minimum of 0.5m from the footway to prevent spilling onto highways. A paved or asphalt strip will be required to separate.

SPECIAL PLACES: STREETS

ROADS



Hot rolled asphalt with natural aggregate chippings

FOOTWAYS



Concrete block paving



Concrete setts

CYCLEWAYS



Hot rolled asphalt



Integrated cycleway symbol



Demarcation block

KERBS



Conservation kerb



Flush conservation kerb

CHANGE OF SURFACE



Concrete setts in contrasting colour
(example use include transition strips)



Granite setts in contrasting colour
(example uses include drainage areas in verges and at junctions)

PARKING



Permeable concrete setts in various colour tones

STREET FURNITURE

As per the material palette, the street furniture selection should be specified to enhance the character of the open space around it as a complement to the surrounding architecture and functionality of the place.

A site-wide strategy of products and styles should be used throughout the development to establish a clear family range and ensure a safely navigable public realm.

A standard Hampshire street furniture palette should be used in relation to the streetscape where products must comply to the adoptable standards set out by the Local Authority and respond to Highways requirements.

Welborne's street furniture strategy is divided into three main approaches: Typical Treatment; Informal Treatment; and Special Places.

TYPICAL TREATMENT



- Palette to convey a timeless and tranquil feel with particular attention to robust and hard wearing materials
- Material example: metalwork stainless steel/galvanised steel

INFORMAL TREATMENT



- Natural and semi-natural, softer feel with particular focus on local handmade crafts and arts
- Material example: timber, stone

SPECIAL PLACES



- High-quality street furniture characterised by a robust, natural style and sustainable use of materials that convey the identity of the place
- Material example: FSC hardwood, natural stone, high-quality concrete, metalwork stainless steel/galvanised steel



6g. SLOW-SPEED STREETS

The street network at Welborne will be designed to achieve slow speed streets through the implementation of regular traffic calming measures to reduce traffic speeds and improve quality of place.

Secondary and tertiary streets across Welborne Garden Village will have a maximum design speed of 20mph (32 kph), though, in accordance with para 8.2.13 of Manual for Streets 2 (MFS2), this will be achieved through design rather than a signposted 20mph zone. Following Hampshire County Council TG3 para 3.1.2 visibility splays and minimum stopping sight distance will be based on the 20mph design speed.

To achieve speeds of 20mph, or less, traffic calming features shall be spaced no more than 70m apart (DfT1, 2007, p. 88). Straight uninterrupted links should therefore not exceed 70m in length.

There are various features which can be used to break up a continuous link and slow traffic. Different traffic calming features work in different ways and with varying degrees of effectiveness.

Traffic calming measures proposed for use across tertiary and secondary streets within Welborne are:

KEY COMPONENTS *(Must be adhered to)*

- Horizontal alignment – bends with less generous radii (CIHT, 2010, p. 52), chicanes, etc.
- Changes in priority
- Carriageway narrowing using changes in material
- Reduced visibility
- Raised junctions using changes in material
- On-street parking, including chevron and perpendicular bays
- Central islands using changes in material

Psychology and perception are important factors in achieving the desired design speed and establishing a sense of place, although they are not permanent traffic calming features. The presence of pedestrians, cyclists and active frontages can have a strong influence on reducing the speed at which people choose to drive (DfT1, 2007, p. 88).

References

CIHT, 2010. *Manual for Streets 2*, London: CIHT.
DfT1, 2007. *Manual for Streets*, London: Thomas Telford.
DfT2, 2007. *LTN 01/07 Traffic Calming*, London: TSO.

I. Horizontal Alignment

Manual for Streets (MFS) and Manual for Streets 2 (MFS2) recognise that the minimum bend radii required in CD109 are not suitable for designing high-quality streets. For streets with a design speed of 60kph and less, MFS2 recommends implementing horizontal curves with radii four steps below the desirable minimum from CD109, having a v^2/R value equal to 56.

These tighter curve radii can be implemented as traffic calming features (CIHT, 2010, p.52) and will naturally create slower speed streets than the design speed.

Table 1 below shows these minimum acceptable carriageway centreline radii for design speeds of 10, 20 and 30mph. For example, on a 20mph road- a V^2/R value of 56 (which is 4 steps below the minimum in CD109; the recommended value for slower speed streets in MfS2), the minimum radius is 18.3m

Table 1:
Percentage speed reduction at bends (CIHT, 2010, p. 52)

Design Speed, V	mph	10	20	30
	kph	16	32	48
MfS2 recommended 4 steps below min. in CD 109, V^2/R		56	56	56
MfS2 recommended radii (using V^2/R)	R (m)	4.6	18.3	41.1



Use of horizontal curve as a traffic calming feature

2. Changes in Priority

Changes in priority at junctions and no defined priority at squares can be introduced as a traffic calming feature to interrupt otherwise continuous links.

Where possible, squares (with no defined priority) should be used in preference to raised junctions.



Square used to introduce change in priority

3. Carriageway Narrowing

Carriageway narrowing can have a significant influence on speeds (DfT1, 2007, p. 88). See Figure 1. Carriageway narrowing at Welborne will be functional and have a purpose such as crossing points. Change of material surfacing will be used to highlight crossing points. Block paving is proposed for non-primary roads and bus routes.

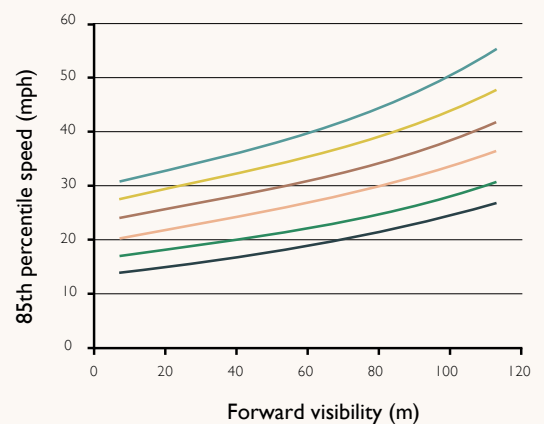
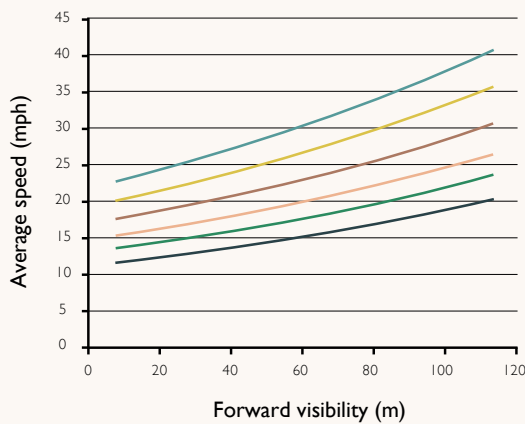
Narrowed sections shall be a minimum width of 3.7m (1.85m lanes) on tertiary streets and shall be 5m (2.5m lanes) on secondary streets.



Carriageway narrowing to reduce traffic speeds

Figure 1:

Correlation between visibility, carriageway width and vehicle speeds (DfT1, 2007, p. 89)



- Road width = 10 m
- Road width = 9 m
- Road width = 8 m
- Road width = 7 m
- Road width = 6 m
- Road width = 5 m

4. Reduced Forward Visibility

As a traffic calming feature, MFS2 permits minimum forward visibility to be restricted below the minimum stopping sight distance (SSD) (CIHT, 2010, p. 75). The minimum SSD for 20mph and 30mph design speeds are 25m and 43m, respectively. Where used in Welborne, restricted visibility should be introduced naturally into the masterplan.



Road alignment with reduced forward visibility that will promote slower vehicle speeds

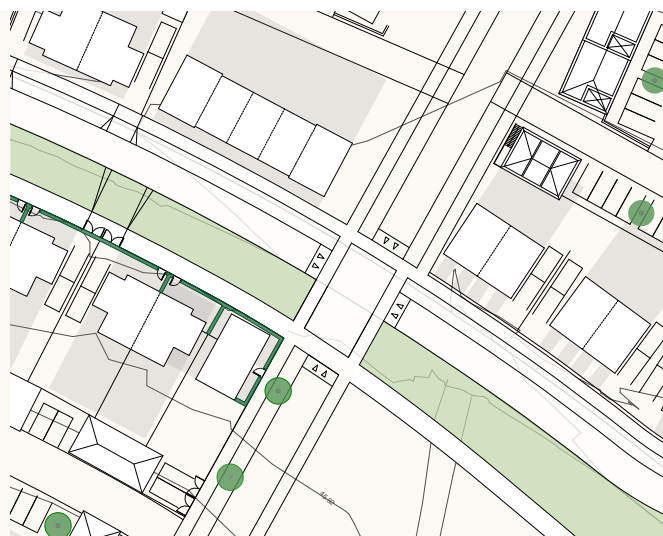


Houses create reduced forward visibility and speed reduction bend

5. Raised Junctions

Raised junctions can be used on cross-roads and T-junctions to provide traffic calming features, but also to highlight and improve the safety of pedestrian crossings. Raised junctions provide very effective reductions in traffic speeds and accidents and injuries, they cause little delay to emergency services and are well perceived by the public (DfT2, 2007, p. 9).

At Welborne raised junctions will be designed with an approach gradient of 1:35 over 3 standard kerb lengths (2745mm). The surface material of ramps and the raised table will be resin bound gravel.



Raised junction at a cross-roads that shall act as a traffic calming feature

6. On-Street Parking

Perpendicular or echelon formation on-street parking can provide effective traffic calming through the introduction of side friction (DfT I, 2007, p. 88). However, longitudinal parking has a reduced traffic-calming impact.

Perpendicular or echelon parking should be provided in discreet groups, with regular gaps, to maintain good access and visibility for pedestrians.



Perpendicular parking at a proposed public space that will function as a traffic calming feature



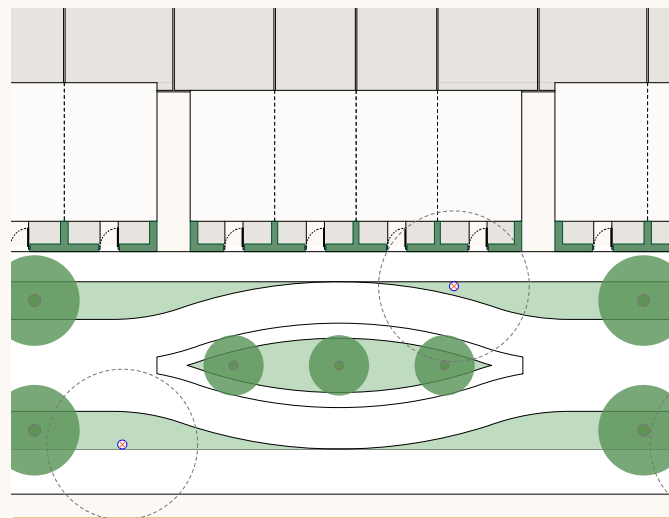
On-street parking to introduce side friction

7. Central Island

Central islands can be provided as traffic calming features on continuous links, and can also provide space for trees and landscaping.

Central islands will be 3m wide with 1m overrun areas on each side (total width 5m), and have 2.75m through lanes. Islands can be developed symmetrically or asymmetrically from the centre line with 1 in 5 tapers.

Any design will need to be tracked for buses and refuse vehicles. Central landscape areas if not adopted by HCC will be the responsibility of WGVT.



Central island feature with tree planting

6h. VISIBILITY

Secondary and tertiary streets across Welborne will have a maximum design speed of 20mph (32 kph). However, the design speed will be dynamic along each street. Reduced forward visibility and tight corner radii will be incorporated into the development to reduce design speed in distinct areas.

A central component of Welborne's ethos will be the presence of a large number of trees throughout the site. Visibility splays, on occasion, will be obstructed by trees; however, this is unavoidable given the nature of the development. Due to the low design speeds throughout the development, isolated obstructions should not pose an unacceptable safety risk. The principle is supported

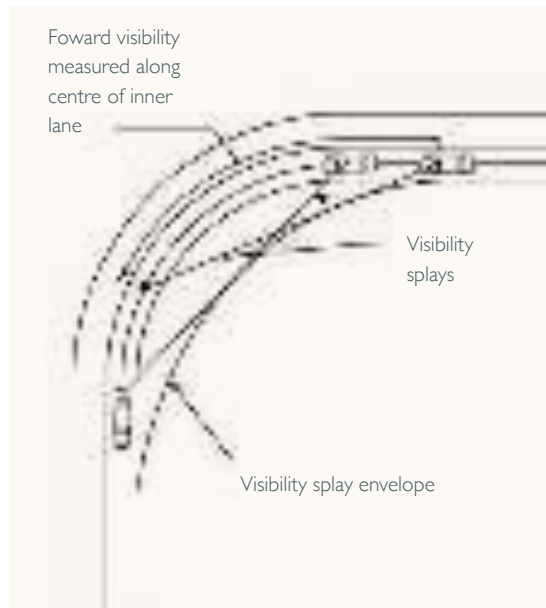
by Manual for Streets, which states 'occasional obstacles to visibility that are not large enough to fully obscure a whole vehicle or a pedestrian, including a child or wheelchair user, will not have a significant impact on road safety' (DfT I, 2007, p. 94).

KEY COMPONENTS *(Must be adhered to)*

This section addresses the visibility principles to be adopted across Welborne in relation to:

1. Forward visibility, horizontal and vertical
2. Junction visibility splays
3. Cycle and pedestrian visibility
4. Frontage access visibility splays

Figure 2:
Measurement of forward visibility (DfT I, 2007, p. 94)



1. Forward Visibility

Forward visibility is the distance a driver needs to see ahead to stop safely for obstructions in the road. The minimum forward visibility is equal to the minimum SSD as required by the design speed, see **Table 2**. It is checked by measuring between points on a curve along the centreline of the inner traffic lane (DfT I, 2007, p. 94). See **Figure 2**.

Reduced visibility can be used as a traffic calming feature, as outlined in below and shown in 5.f 4.

Where tight curve radii or other traffic calming features are used to reduce the local design speed, the minimum SSD can be reduced accordingly. For example, a curve radii of 13m within a 20 mph zone would reduce the required SSD to 18m, a 5m radii curve in a 10 mph zone would reduce the required SSD to 9m, see **Table 1**.

Figure 3:
Vertical visibility envelope

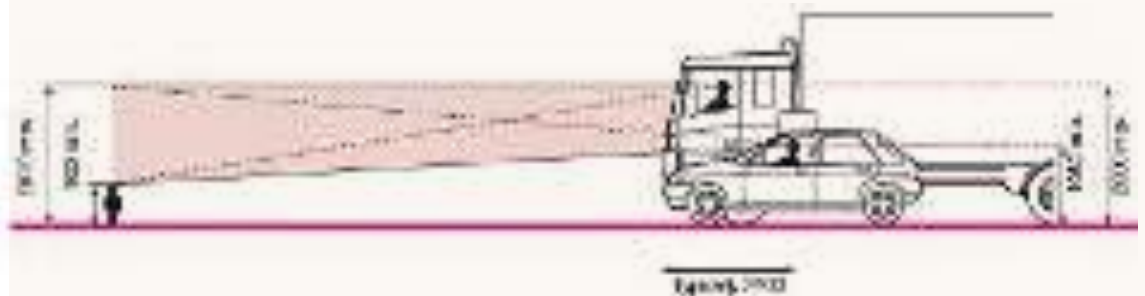


Table 2:

SSD to be adopted across Welborne

Design Speed (Kph/mph)	SSD (m)
16 / 10	11
32 / 20	25
48 / 30	43

Visibility should not be obstructed by vertical obstructions within an envelope 0.6m above the carriageway level to 2m above carriageway level, see **Figure 3**. Boundary treatments adjacent to the carriageway shall be designed accordingly.

2. Junction Visibility

Junction visibility splays ensure there is adequate inter-visibility between vehicles. See **Figure 4**. Within Welborne an X distance of 2.4m shall be used, and the Y distance shall be equal to the minimum SSD as per **Table 2**.

Due to the nature of Welborne, there may be instances where trees fall within junction visibility splays (see above. DfT 1, 2007, p. 94).

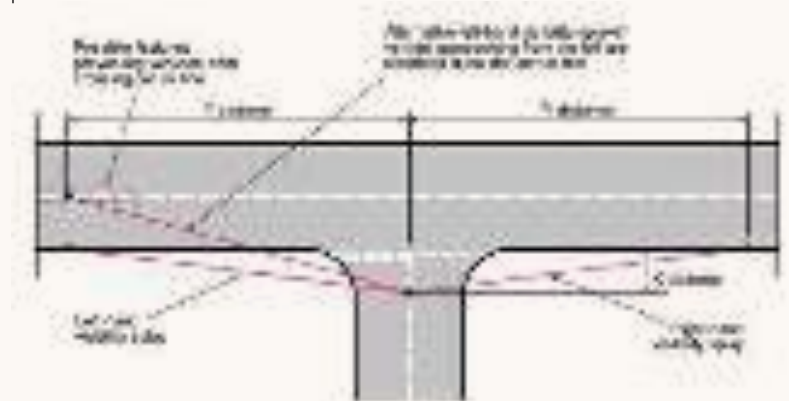


Figure 4:

Junction visibility splay

3. Cycle, pedestrian and equestrian visibility splays

Welborne will promote active travel across the site and the required infrastructure shall be designed into the masterplan. Where non-motorised user (NMU) routes meet the carriageway, visibility splays shall be provided to improve the safety of non-motorised users.

Visibility splays shall be designed according to **Figure 4**, with the X distance measured from the carriageway kerb/channel line. Y distances shall be set according to the carriageway design speed and comply with **Table 2**.

X distances across the development shall be;

- Pedestrians – 1.5m
- Cycles – 2.5m
- Equestrians – 3m

Figure 5:

Typical Welborne frontage access

4. Frontage access and visibility

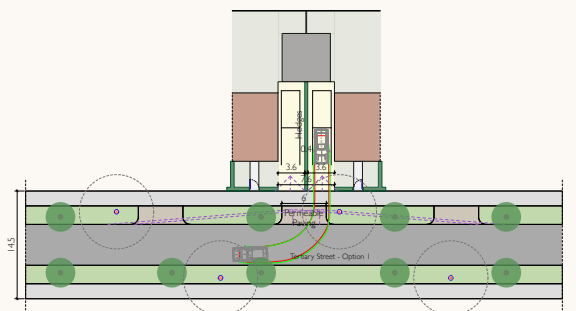
Frontage accesses at Welborne will usually cross a footway and/or cycleway route before reaching the carriageway. See **Figure 5**.

carriageway kerb/channel line, Y distances shall be set in accordance with the design speed.

See **Figure 5**.

A 2m x 2m inter-visibility zone 0.6m above carriageway level shall be provided from the rear of the footpath/shared space to ensure drivers exiting driveways have good visibility of approaching pedestrians or cyclists, see **Figure 5**.

Visibility splays shall be provided per **Figure 4** with an X distance of 2m measured from the



References

CIHT, 2010. *Manual for Streets 2*, London: CIHT.
 DfT 1, 2007. *Manual for Streets*, London: Thomas Telford.
 DfT 2, 2007. *LTN 01/07 Traffic Calming*, London: TSO.

6i. PARKING & CYCLE PARKING STRATEGY

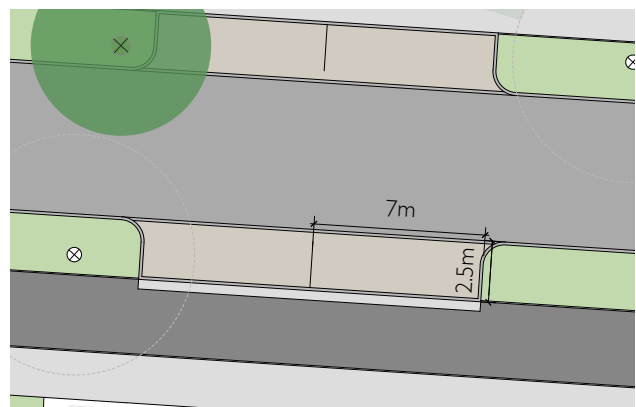
PARKING AND CYCLE STANDARDS

The residential and commercial parking and cycle parking strategy is set out in the Welborne Strategic Design Code and will conform with FBC Parking Standards and key requirements as set out in Welborne Design Guidance by FBC.

Visitor parking will be provided within verges on all street types, in courtyard lanes and parking courts and public parking areas.

KEY COMPONENTS *(Must be adhered to)*

- Standard parking spaces will be 5m x 2.5m (4.8m x 2.4m is considered unacceptable at Welborne)
- Perpendicular parking spaces will be 2.5m x 6m, where provided in a verge the spaces on the end of runs will be 2.5m x 7m to enable a 90-degree kerb return.
- Communal EV charge points will be provided in the Village Centre, District Centre and other commercial and public parking areas. These will be located in grouped spaces with a charging hub. There will be no EV charging points on typical streets.
- Cycle parking will be provided in accordance with FBC Parking Standards. Cycle parking will be available throughout all public areas in Welborne, particularly in areas of high footfall such as the District Centre and Schools. Cycle parking locations should be well lit, have high levels of natural surveillance, be as close as possible to the destination and allow for bike locking.
- Parking provision for schools should be assessed in relation to the agreed travel plan for each location, and again should be kept to a minimum to help reinforce sustainable and healthy modes of movement to the schools. Drop-off and pick-up areas for vehicles should be discouraged, but details for each site are to be agreed with FBC and HCC.
- Two car parks for access to the SANGs will be provided, one off Knowle Road on the western boundary of the site to access the Welborne Mile and Dashwood to the north, and a second off Funtley Hill for access to Fareham Common and the Welborne Mile. These will include provision for horse box parking.



In verge parking space 2.5m x 7m with a 90 degree kerb return and change of surface paving slab strip where parking abuts the cycleway



The location and design of non-residential parking shall be an important consideration in the design of streets at Welborne. It can be accommodated as bays within verges, in public spaces or in perpendicular arrangements



7. STREET LAYOUT

7a. PRINCIPAL STREET TYPES

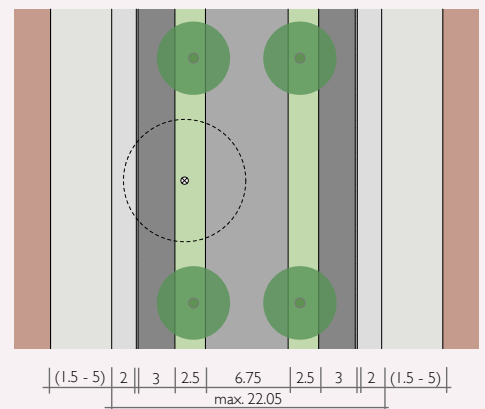
The patterns of movement that are laid out by streets often have a longevity far greater than the buildings that front onto the spaces. Hence great care shall be taken in the design and layout of the streets. This section sets out the key criteria for street design at Welborne. Each street form is described in terms of design criteria, typical dimensions, cross section, plan form and precedent examples.

The street network at Welborne comprises of typical streets as illustrated in the matrix of types opposite. In addition to these standard streets and paths are the bespoke conditions of Welborne Way and Knowle Road. These routes are two of the most important movement connections within Welborne.



WELBORNEWAY

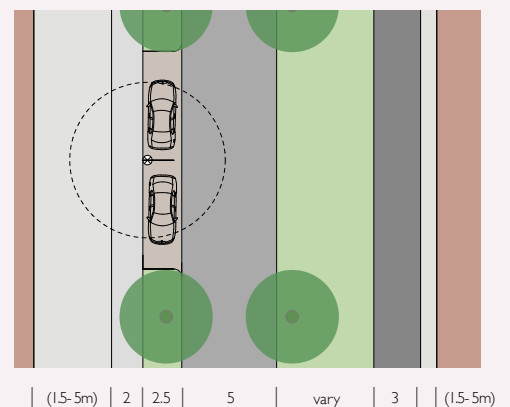
Welborne Way is the principal north-to-south movement route through Welborne, providing a connection to the M27. It shall be important to consider its role both as a vehicular movement route but also one that is conducive to pedestrian and cycle movements.



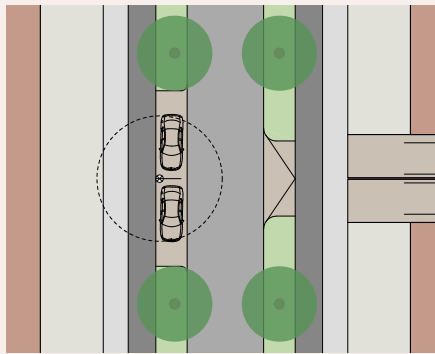
KNOWLE ROAD

Knowle Road is an existing 50mph road connecting Knowle village to the Wickham Road.

Knowle Road will take on a new character as a residential street with a reduced speed limit of 30mph.

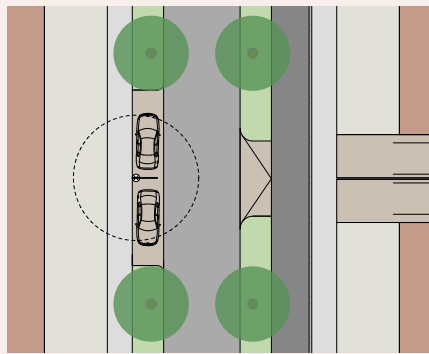


PRINCIPAL STREET TYPES USED ACROSS WELBORNE



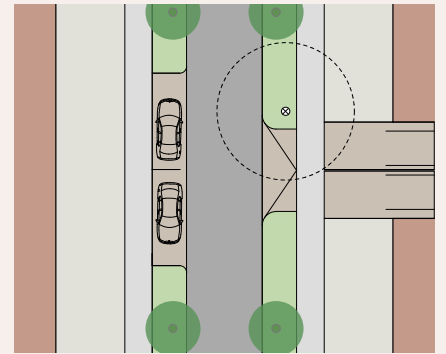
|(1.5 - 5)| 2 | 2 | 2.5 | 6.1 | 2.5 | 2 | |(1.5 - 5)|
max. 19.5

PRIMARY STREET



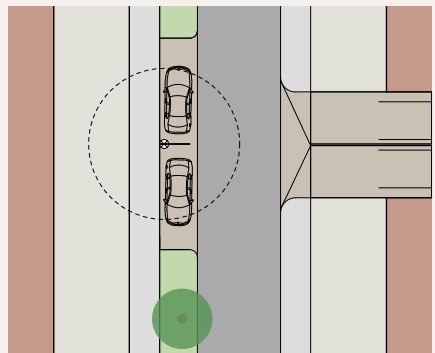
|(1.5 - 5)| 2 | 2.5 | (5.5 - *6.1) | 2.5 | 3 | 2 | |(1.5 - 5)|
max. 18.3

SECONDARY STREET
(*6.1 ONLY FOR BUS ROUTES)



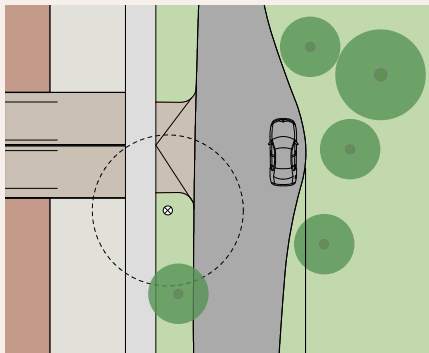
|(1.5 - 5)| 2 | 2.5 | (4 - 5.5) | 2.5 | 2 | |(1.5 - 5)|
max. 14.5

TERTIARY STREET - OPTION 1



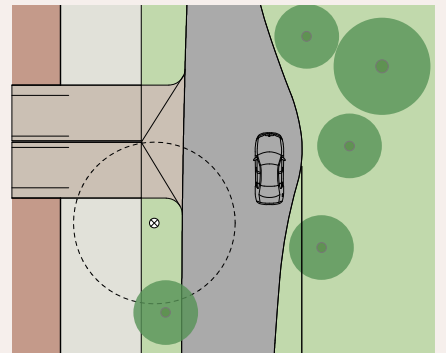
|(1.5 - 5)| 2 | 2.5 | 4 - 5.5 | 2 | |(1.5 - 5)|
max. 12

TERTIARY STREET - OPTION 2



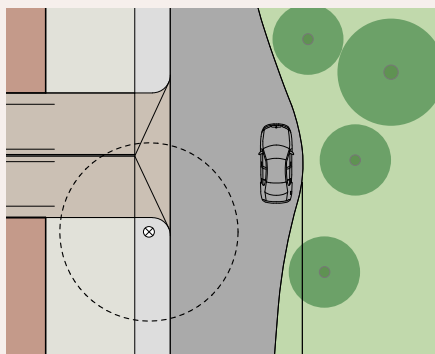
|(1.5 - 5)| 2 | (0.5 - 2.5) | (3.7 - 7.5)|
max. 12

EDGE LANE - OPTION 1



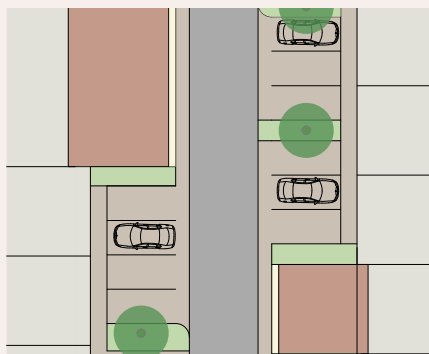
|(1.5 - 5)| (0.5 - 2.5) | (3.7 - 7.5)|
max. 10

EDGE LANE - OPTION 2



|(1.5 - 5)| 2 | (3.7 - 7.5)|
max. 12

EDGE LANE - OPTION 3



7.2 | 5 | 7.2 |
max. 19.4

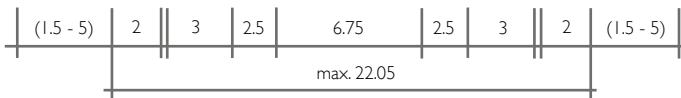
COURTYARD LANE

KEY

- Building block
- Privacy strip/front garden
- Footpath
- Segregation strip
- Cycleway
- Shared cycleway
- Tree verge
- Street lamp
- Tree
- Carriageway
- Parking

All measurements in metres

WELBORNE WAY

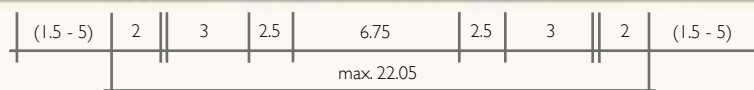


All measurements in metres

Welborne Way is the main north–south route through Welborne. Its characteristics will include:

- a generous carriageway with dedicated cycleways in both directions
- a tree verge and footway on both sides
- taller buildings between three- to five-storeys high fronting onto the road
- a tree verge and footway on both sides with tree and planting proposal compatible with SuDS system. Refer to Strategic Design Code for details.

Welborne Way will connect and pass through a number of the principal centres within the settlement including the Village Centre and District Centre. These are opportunities for 'special' junctions and places that emphasise the urban settings.





STREET TYPE: WELBORNE WAY

Character	Principal movement route, wider, busier, dedicated cycle facilities, taller buildings.
Design speed	30 mph

DESIGN ELEMENTS

Desired Radii	6.0m (vehicle tracking to be used)
Trees	In verge
Lighting	✓
Bus route	✓
On-street car parking	None
Traffic calming	Junction spacing, squares, bends, positioning of buildings and trees, activity
Utilities	Under footway
Drainage channels	At edge

MATERIALS

Materials palette	Typical treatment
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WELBORNE WAY

LINK CAPACITY

A VISSIM micro simulation model has been prepared to provide data on the traffic implications within the internal Welborne highway network, specifically Welborne Way.

The coding of the VISSIM model has considered the required street width of Welborne Way of 6.75m. Outputs from the model have confirmed that users of Welborne Way would not experience material delays and free flow of traffic would not be compromised.

Examples of the average speeds of traffic using Welborne Way during peak times have been identified using 'Heat Maps' extracted from the VISSIM model. Yellow sections of the Heat Map indicate average vehicle speeds of 30 mph. Given the absence of significant junctions formed with Welborne Way, the majority of Welborne Way will see average traffic speeds of 30 mph being achieved, confirming that sufficient link capacity will be available.

FRONTAGE ACTIVITY

There will be frontages along Welborne Way to provide a high-quality urban environment with a strong sense of place. The frontages will provide good passive surveillance and encourage active travel along the length of Welborne Way. Frontages will mostly be residential. However, there will be distinct areas of leisure and commercial units, in areas close to the Village Centre to the north of Welborne Way, and the District Centre to the south. No further new access points will be facilitated from these centres onto Welborne Way. The centres will be serviced either through access points currently proposed on Welborne Way or by the wider street network.

However, there will be minimal direct accesses onto Welborne Way, as this would compromise the traffic capacity and lead to interruptions of the proposed footway and cycleways.

PARKING RESTRICTIONS

Double yellow lines (Dia 1018.1) will restrict on-street parking along Welborne Way. The gap between the edge of the carriageway and the nearest edge of the longitudinal line shall be approximately 250mm. The lines will be 50mm wide, with a 50mm gap between the lines; they will be painted in No 353 (Deep Cream).



Right: No 353 (Deep Cream)

JUNCTION LOCATIONS AND DESIGN

The main junction on Welborne Way is at the intersection with Dashwood Avenue.

Other junctions along the route shall be priority junctions with passing bays per CD123 Figure 6.25N2.

See Section 7 for diagrams of typical junctions on Welborne Way.

MINIMUM CENTRELINE CURVE RADII

The design speed for Welborne Way will be 30 mph/48 kph; the minimum centreline curve radii along Welborne Way will be 41m, as per Table 8.1 of Manual for Streets 2.

FOOTWAY AND CYCLEWAY DETAILS

To encourage active transport, Welborne Way will have a segregated two-way cycle and pedestrian route on both sides of the carriageway. A 150mm channel block will provide segregation between the pedestrian and cycle routes.



Above: Visualisation of Welborne Way

Segregated routes will be provided in preference to shared space routes as they are safer for visually impaired users and young children; they also provide better facilities for cyclists.

Cycleways will be 3m wide, to accommodate two-way cycle traffic, recumbents and child's trailers.

Footways will be 2m wide to allow buggies or wheelchairs to pass each other.

PUBLIC TRANSPORT PROVISION

There will be a proposed bus route along Welborne Way. To avoid interrupting traffic flows, bus stops shall be provided in lay-bys. The Bus Rapid Transit (BRT) route will run along Welborne Way. However, no specific BRT infrastructure will be required to facilitate this, as the route provides sufficient traffic capacity and queuing is not expected.

WELBORNE WAY

PHASING AND DELIVERY

Welborne Way will be delivered in 2 phases

Phase 1 - Welborne Way (Knowle Road to Broadway) Works - the works consisting of the provision of a vehicular link between Knowle Road and Broadway (to include the construction of Welborne Way and the construction of a junction at Knowle Road and the construction of the vehicular link to Broadway) as shown in principle on plan 7328_104 Rev D and as agreed in accordance with any Highways Agreement;

Phase 2 - A32/Welborne Way Roundabout Works
- the provision of a permanent vehicular roundabout

access onto the A32 as shown in principle on the plan 6091/GA/311 Rev G including the shared use route extension on the southern arm to the "A32 / Knowle Road / Chalk Lane Roundabout Works and as agreed in accordance with any Highways Agreement.

It is important that these junctions are delivered in a timely manner as their main purpose is to alleviate congestion and delays on the A32. Although the transport assessment demonstrated the Welborne Way is only required prior to the occupation of 1,851 units and the roundabout by 2,601 units Buckland, as master developer, will monitor the capacity of Knowle Road and Broadway Roundabout annually from the

Phase 1 - Welborne Way (Knowle Road to Broadway) Works



occupation of 1,160 units. In the event that the RFC values exceed 0.75 on any one arm of the Knowle Road and Broadway Roundabouts, WLL will complete the construction of Welborne Way between Broadway Roundabout to the south and Knowle Road to the north within 12 months. If the RFC values do not exceed 0.75 Buckland will complete the road prior to

- Occupation of 1851 Residential Units within Area X; or
- Occupation of any Residential Unit or Commercial Unit within Area Y; or
- More than 25% Occupation of any land use with the Employment Area;

Once Welborne Way is open to traffic Buckland will continue to monitor Knowle Road and Broadway Roundabout, in the event the RFC values exceed 0.75 on any one arm of the Knowle Road and Broadway Roundabouts, WLL will commence the construction of Welborne Way Roundabout within 6 months. In order to safeguard the earlier delivery of the roundabout, if required, Buckland will submit a S278 preliminary design check prior to the occupation of the 700th units. If the RFC values do not exceed 0.75 Welborne Way Roundabout will be complete prior to the occupation of 2,601st unit

Phase 2 - A32/Welborne Way Roundabout Works



WELBORNE WAY: PLANTING

Effect / Character	<ul style="list-style-type: none"> • Formal avenue tree planting with native grassland rich in local wildflora • Continous, consistent character through three Landscape Character Areas
Arrangement	<ul style="list-style-type: none"> • Trees located centre to the verge strip at regular intervals • Specimen trees placed at junctions or in strategic locations shall enhance and frame views • Native species-rich grassland beneath • Street light columns distance to be min. 5m from tree stem. Refer to lighting column guidance • Tree planting to provide a continuous canopy effect and enhance both pedestrain and cyclist experience
Tree type	<ul style="list-style-type: none"> • Trees to have neat, conical crown canopy shape • Large tree species over 10m • Tree species compatible with SuDS system where located within swales • Semi-mature single clear-stem species (min 2.1m) to avoid visibility issues and clashes with vehicles • Seasonal interest trees as landmak trees are supported in association with footway junctions and resting places
Verge Types	<ul style="list-style-type: none"> • Species-rich grassland with diverse native wildflora to provide functional biodiversity gain and aesthetic benefit for street character and quality. The diverse grassland overlies a permeable tree planting zone as a continuous trench that will support healthy establishment and long-term growth. Careful placement of root directing barrier will protect below-ground utilities. Surface water will attenuate via the root zone by the sensitively integrated drainage design, enabling vegetation to support silt and nutrient level reduction. from surface water run-off. The cutting regime will maintain a high-quality appearance, for higher infiltration rates and to encourage species diversity



1. Tree planting
2. Verge grassland
3. Private hedgerows

INDICATIVE STREET TREES



1. *Quercus palustris*
2. *Platanus x hispanica*
3. *Fagus sylvatica*

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
<i>Quercus palustris</i> (Pin Oak, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
<i>Platanus x hispanica</i> (London Plane, Large)	35-45	
<i>Fagus sylvatica</i> (Beech, Large)	35-45	

INDICATIVE LANDMARK TREES



1. *Liquidambar styraciflua*
2. *Liriodendron tulipifera*
3. *Quercus robur*

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
<i>Liquidambar styraciflua</i> (Sweet Gum, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
<i>Liriodendron tulipifera</i> (Tulip Tree, Large)	35-45	
<i>Quercus robur</i> (Pedunculate Oak, Large)	35-45	

INDICATIVE VERGE GRASSLAND

SPECIES MAY INCLUDE

A carefully selected combination of finer, low-growing grass species as an open matrix for wildflora:

- Birds-foot-trefoil, Buttercup
- Clover
- Cowslip
- Eyebright
- Knapweed
- Lawn Chamomile
- Ragged Robin
- Selfheal
- Yarrow

Species selection to respond to Landscape Character Area

KNOWLE ROAD



Knowle Road is a one-off secondary street. It is based on the alignment of the existing road, with some realignment. The existing 50mph speed limit will be lowered. It retains existing planting where possible and introduces additional tree planting. Planting should celebrate the Woodland Landscape Character Area.

Verge planting includes existing grassland planting where retention is possible on the southern verge with lawn and meadow planting on the northern verge.



Note: This is an existing shared cycle/pedestrian route

All measurements in metres





STREET TYPE: KNOWLE ROAD

Character	Woodland and Downland Landscape Character Areas
Design speed	Exiting 50mph to be lowered

DESIGN ELEMENTS

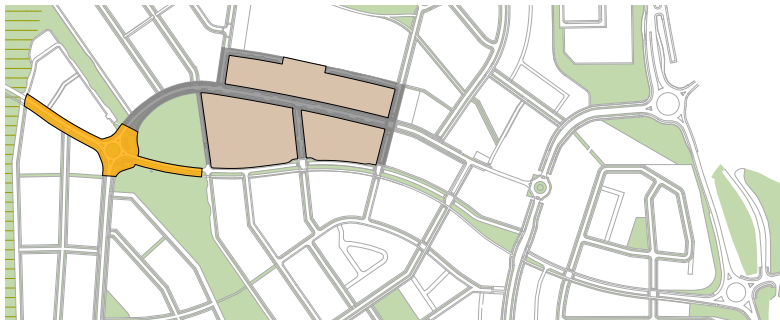
Desired Radii	6.0m
Trees	In verge
Planting character	Bespoke character incorporating existing planting. Planting responds to Landscape Character Area
Lighting	✓
Bus route	N/A
On-street car parking	Parallel
Traffic calming	Junction spacing, squares, positioning of buildings and trees, activity
Utilities	Existing and proposed under footway and cycle route
Drainage channels	At edge

MATERIALS

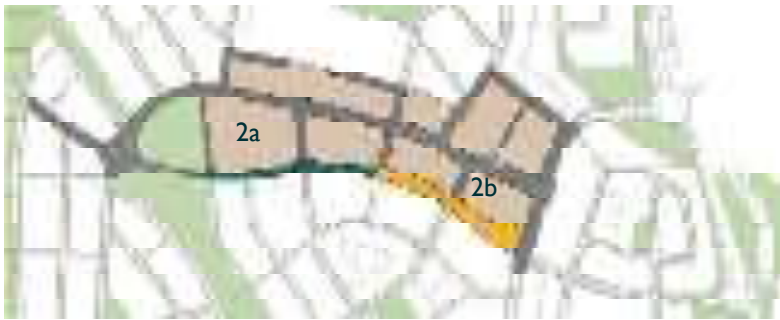
Materials palette	Typical treatment
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KNOWLE ROAD

PHASING AND DELIVERY



Phase 1 – Construction works to Knowle Road from the western site boundary to Welborne Park Roundabout. This section of Knowle Road to remain 6.7m in width.



Phase 2 – Construction works to Knowle Road from Welborne Park to Welborne Way. Road width reduced to 5.5m. 2a delivered with adjacent residential units 2b delivered with Village Centre.



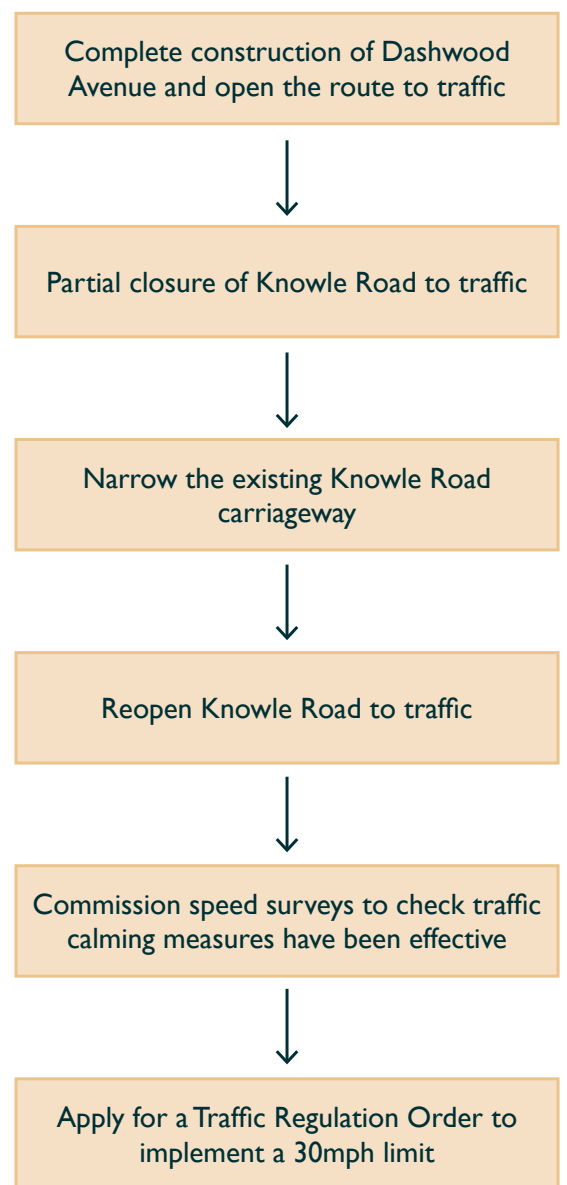
Phase 3 – Construction works to Knowle Road from Welborne Way to A32. Road width reduced to 5.5m.

SPPED CONTROL CAPACITY

To reduce the travelling speed along Knowle Road it shall be narrowed to 5.5m.

The provision of the raised junctions will slow traffic.

Where Knowle Road crosses Welborne Way, the junctions will be staggered to reduce conflict at the crossing. The cycleway and footpath shall remain on the current and most direct alignment.



KNOWLE ROAD: PLANTING



1. Northern verge: Formal tree spacings, Medium street trees with landmark trees at junctions or ends of parking bays. Use of smaller ornamental multistem trees and hedge species within understory verge
2. Southern verge: Informal spacings, individual trees, clusters and groups, retaining existing trees and shrub planting where possible, inclusion of rich understory of hedge/shrub species and flowering and ornamental multistems
3. Existing planting retained where possible
4. Additional tree planting within retained grassland area
5. Proposed verge and planting to respond to Woodland Landscape Character Area
6. Hedgerows species to respond to Woodland Landscape Character Area

INDICATIVE TREES	SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
	Northern verge: Street trees		
	Acer campestre (Field Maple, Medium)	30 - 40	26m ³ of Stockholm System, which is equivalent to 20m ³ of Topsoil
	Alnus cordata (Italian Alder, Medium)	30 - 40	
	Ulmus 'New Horizon' (resistant Elm, Medium)	30 - 40	
	Northern verge: Landmark and ornamental street trees		
	Juglans regia (Walnut, Large)	30-40	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
	Quercus robur (Pedunculate Oak, Large)	30-40	
	Tilia cordata (Lime, small-leaved, Large)	30-40	
	Southern verge: Street trees		
	Existing retained trees and shrubs		
	Acer campestre (Field Maple)		
	Prunus Avium (Wild Cherry)		
	Corylus avellana (Hazel)		
	Quercus robur (Oak)		

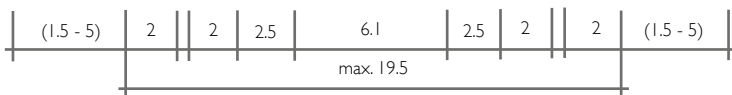
PRIMARY STREETS



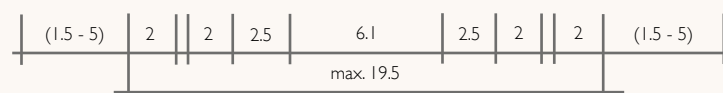
Primary streets provide the main green infrastructure within the masterplan. They are the widest streets, with footways and tree verges on either side and taller buildings than on other routes.

These streets are planted with large single tree species selected for their resilience and provision of important habitat. Large canopies will soften and integrate the development within the landscape.

Primary streets will include one way cycle lanes on separate sides of the street.



All measurements in metres





STREET TYPE: PRIMARY STREETS

Character	More formal design, constant cross section, larger trees, important routes
Design speed	20 mph

DESIGN ELEMENTS

Desired Radii	6.0m (vehicle tracking to be used)
Trees	In verge
Lighting	✓
Bus route	N/A
On-street car parking	Parallel or chevron (unmarked)
Traffic calming	Junction spacing, squares, bends, positioning of buildings and trees, activity
Utilities	Under footway
Drainage channels	At edge

MATERIALS

Materials palette	Typical treatment
-------------------	-------------------

PRIMARY STREETS

Effect / Character	<ul style="list-style-type: none"> • Formal tree planting with verge planting • Continous consistent character through 4no. landscape character areas
Arrangement	<ul style="list-style-type: none"> • Trees located centre to the verge strip at regular intervals • Single species throughout • Specimen trees placed at junctions or in strategic locations shall enhance and frame views
Tree type	<ul style="list-style-type: none"> • Tall, large tree species over 10m • Trees to have neat, conical crown canopy shape • Single clear-stem species are supported to avoid visibility issues • Semi-mature types are encouraged to balance with the height of proposed buildings along this street type • Flowering and fruiting trees as landmark trees are supported in association with secondary street junctions • Tree planting to avoid fruiting tree species if associated with on-street parking underneath
Verge Type	<ul style="list-style-type: none"> • Species-rich grassland with diverse native wildflora to provide functional biodiversity gain and aesthetic benefit for street character and quality. The diverse grassland overlies a permeable tree planting zone as a continuous trench that will support healthy establishment and long-term growth. Careful placement of root directing barrier will protect below-ground utilities. Surface water will attenuate via the root zone by the sensitively integrated drainage design, enabling vegetation to support silt and nutrient level reduction. from surface water run-off. The cutting regime will maintain a high-quality appearance, for higher infiltration rates and to encourage species diversity



INDICATIVE STREET TREES



1. *Carpinus betulus*
2. *Corylus colurna*
3. *Fagus sylvatica*
4. *Tilia cordata*

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
<i>Carpinus betulus</i> (Hornbeam, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
<i>Corylus colurna</i> (Turkish Hazel, Large)	35-45	
<i>Fagus sylvatica</i> (Beech, Large)	35-45	
<i>Tilia cordata</i> (Small Leaf Lime, Large)	35-45	

INDICATIVE LANDMARK TREES



1. *Juglans regia*
2. *Liriodendron tulipifera*
3. *Pinus sylvestris*

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
<i>Juglans regia</i> (Walnut, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
<i>Liriodendron tulipifera</i> (Tulip Tree, Large)	35-45	
<i>Pinus sylvestris</i> (Scots Pine, Large)	35-45	

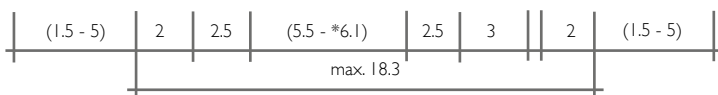
Species selection to respond to Landscape Character Area

SECONDARY STREETS

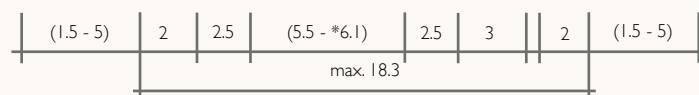


Secondary streets are the arteries that link the busier primary street network to the heart of each neighbourhood, providing key routes for the tertiary streets and edge lanes to connect into.

Planting will reflect and celebrate the Landscape Character Areas within which the secondary street is located and the existing soil conditions. Verge planting will be predominantly lawn with the option of meadow planting and highlight planting that may be used to enhance spaces such as key junctions, public buildings and school entrances.



All measurements in metres. *6.1 only for bus routes





STREET TYPE: SECONDARY STREETS

Character	Human scale, tree lined, low traffic volume and speed residential areas.
Design speed	20 mph

DESIGN ELEMENTS

Desired Radii	2.0m (vehicle tracking to be used)
Trees	In verge
Planting character	Responds to 4no. Landscape Character Areas.
Lighting	✓
Bus route	N/A
On-street car parking	Parallel (unmarked)
Traffic calming	Junction spacing, squares, bends, positioning of buildings and trees, activity
Utilities	Under footway
Drainage channels	At edge

MATERIALS

Materials palette	Typical treatment
-------------------	-------------------

INDICATIVE STREET TREES ALONGSIDE FOOTWAYS



1. Acer campestre
2. Alnus cordata
3. Ulmus 'New Horizon'

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
Acer campestre (Field Maple, Medium)	30-40	26m ³ of Stockholm System, which is equivalent to 20m ³ of Topsoil
Alnus cordata (Italian Alder, Medium)	30-40	
Ulmus 'New Horizon' (Resistance Elm, Medium)	30-40	

INDICATIVE STREET TREES ALONGSIDE CYCLEWAYS

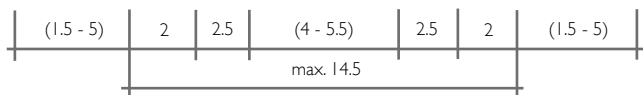


1. Carpinus betulus
2. Fagus sylvatica
3. Tilia cordata

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
Carpinus betulus (Hornbeam, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
Fagus sylvatica (Beech, Large)	35-45	
Tilia cordata (Small Leaf Lime, Large)	35-45	

Species selection to respond to Landscape Character Area

TERTIARY STREETS 1

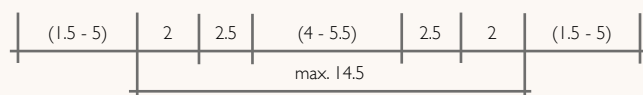


All measurements in metres

Tertiary streets are the most common street type at Welborne and will primarily be low-trafficked residential streets. The carriage way width can vary, depending on the status of the street and intended character.

Access to and from houses plays an important role in the design of tertiary streets. The design needs to both establish a sense of privacy and sense of community. Their ability to contribute to placemaking will be crucial to the development's success.

Tertiary streets will use similar planting types as secondary streets to maintain visual consistency and enhance Landscape Character Areas. They will use a wider variety of smaller tree species, particularly flowering, fruiting and nut-bearing varieties of benefit to wildlife. Planted verges provide an opportunity to maintain the distinctiveness of each Landscape Character Area via wild flower and grass mixtures. Variation in planting treatment for private front gardens shall also reflect Landscape Character Areas. Species will complement fence systems or retaining walls. Where used as private boundary treatments, hedgerows will respond to visibility and safety requirements when associated with access and egress to private dwellings.





STREET TYPE: TERTIARY STREETS

Character	Human scale, tree lined, low traffic volume and speed residential areas
Design speed	20 mph

DESIGN ELEMENTS

Desired Radii	2.0m (vehicle tracking to be used)
Trees	In verge
Planting character	Responds to 4no. Landscape Character Areas
Lighting	✓
Bus route	N/A
On-street car parking	Parallel, intermittent within verge (unmarked)
Traffic calming	Junction spacing, squares, bends, positioning of buildings and trees, activity
Utilities	Under footway
Drainage channels	At edge

MATERIALS

Materials palette	Typical treatment
-------------------	-------------------

INDICATIVE STREET TREES



1. Acer campestre
2. Alnus cordata
3. Ulmus 'New Horizon'

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
Acer campestre (Field Maple, Medium)	30-40	26m ³ of Stockholm System, which is equivalent to 20m ³ of Topsoil
Alnus cordata (Italian Alder, Medium)	30-40	
Ulmus 'New Horizon' (Resistance Elm, Medium)	30-40	

INDICATIVE LANDMARK TREES

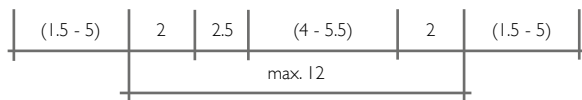


1. Juglans nigra
2. Maytenus boaria

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
Juglans nigra (Black Walnut, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
Maytenus boaria (Mayten, Large)	35-45	

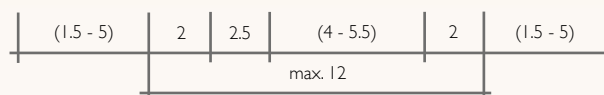
Species selection to respond to Landscape Character Area

TERTIARY STREETS 2



Tertiary streets 2 are a variant of the tertiary street type that has a grassed verge to only one side of the street, with a single line of tree planting. It shall be used for lower-status streets across Welborne.

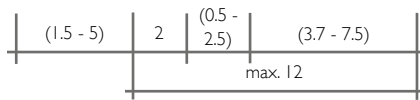
All measurements in metres





Example of tertiary street 2 type

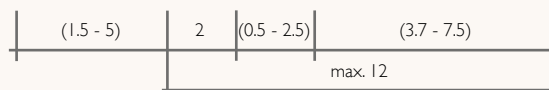
EDGE LANES 1



All measurements in metres

Edge lanes are found on the edges of the Garden Village. They are the interface between the development and open spaces, such as along the northwestern edges facing the Welborne Mile or Dashwood. For these types of road, there is likely to be:

- A carriageway capable of accommodating a single lane of traffic in either direction with a footway on one side
- Homes and private front gardens fronting on to the carriageway on both sides, or on just one side with open space on the other side





STREET TYPE: EDGE LANES

Character	Shared routes for all modes on green edges of Welborne. Low speed, variable width, level surface, informal parking.
Design speed	10 mph

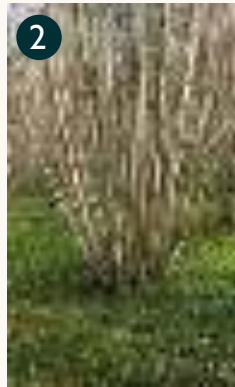
DESIGN ELEMENTS

Desired Radii	N/A
Trees	At edges
Planting character	Responds to 4no. Landscape Character Areas
Lighting	To be agreed based on location and Landscape Character Area
Bus route	N/A
On-street car parking	Parallel, informal (unmarked)
Traffic calming	Narrowing to 3.7m, car parking
Utilities	In adjacent green space
Drainage channels	At edge

MATERIALS

Materials palette	Typical treatment
-------------------	-------------------

INDICATIVE STREET TREES ALONGSIDE FOOTWAYS



1. Acer campestre
2. Corylus avellana
3. Sorbus torminalis

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
Acer campestre (Field Maple, Medium)	30-40	26m ³ of Stockholm System, which is equivalent to 20m ³ of Topsoil
Corylus avellana (Hazel Coppice, Medium)	30-40	
Sorbus torminalis (Wild Service Tree, Medium)	30-40	

INDICATIVE STREET TREES ALONGSIDE CYCLEWAYS

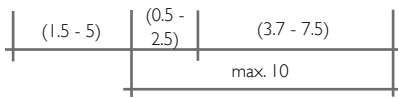


1. Quercus robur
2. Fagus sylvatica
3. Tilia cordata

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
Quercus robur (Pedunculate Oak, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
Fagus sylvatica (Beech, Large)	35-45	
Tilia cordata (Small Leaf Lime, Large)	35-45	

Species selection to respond to Landscape Character Area

EDGE LANES 2



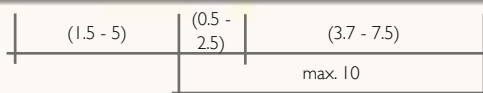
All measurements in metres

The planting character of edge lanes should respond to the Landscape Character Area it is located within and the open space it is fronting.

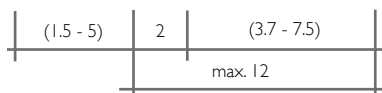
Tree verges may be combined with parking spaces. Trees within verges may vary between single to multi-stem, and will use smaller species corresponding to the Landscape Character Area.

Trees may be planted at irregular intervals to offer an informal, looser character. (For trees within open space refer to the Strategic Design Code.)

Boundary treatments vary between Landscape Character Areas. These may be hedgerows, fencing or earth mounding. They should be appropriate for the anticipated use of the adjacent open space.



EDGE LANES 3

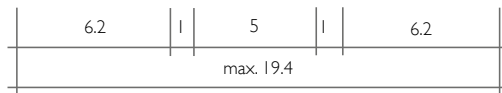


All measurements in metres





COURTYARD LANES



All measurements in metres

Courtyard lanes provide a secondary network of movement routes. They are shared surface streets providing vehicular and parking access to the rear of properties. The design should prioritise pedestrian movement.

Courtyard lanes shall be used by refuse vehicles for collections and so the lanes need to be designed and vehicle tracked to accommodate this.

It shall be important that sufficient space is allowed for the inclusion of trees and planting areas. This shall be used to break up runs of parking and rear boundaries and to make the lanes enjoyable routes for pedestrians and cyclists.





STREET TYPE: COURTYARD LANES

Character	Informal combining residential parking and pedestrian and cycle movement routes.
Design speed	20 mph

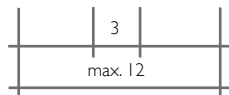
DESIGN ELEMENTS

Desired Radii	6.0m (vehicle tracking to be used)
Trees	Between parking spaces, adjacent to boundary walls
Planting character	Responds to 4no. Landscape Character Areas
Lighting	✓
Bus route	N/A
On-street car parking	Allocated private parking with some visitor parking
Traffic calming	Bends, narrowings, positioning of buildings, trees, activity
Utilities	In designated service margin
Drainage channels	At edge or centrally

MATERIALS

Materials palette	Typical treatment
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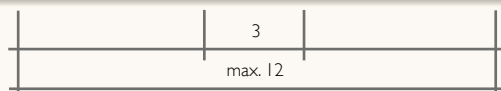
GREENWAYS



All measurements in metres

Greenways provide multifunctional, continuous green routes through the development. They are a key part of the green infrastructure that:

- Provide site-wide strategic east–west non-vehicle infrastructure for pedestrians, cyclists and horse riders
- Incorporate existing, diverted or upgraded Public Right of Ways
- Contain medium–large tree planting to break up the visual mass of the development when viewed from the south
- Generally have development either side
- Incorporate neighbourhood play and fitness trails, as per the Play Strategy
- Provide a range of natural habitats and continuous corridors for wildlife





- Vary in width to accommodate uses and facilities (min. 12m)
- Prioritise safety, through lighting (where appropriate) and natural surveillance from neighbouring uses
- Be easily accessed at regular intervals without barriers
- Provide resting points

The planting character should respond to the Landscape Character Area it is located within, with a focus on native and natural planting. To achieve this, planting may include species identified in the Strategic Design Code planting appendix.

STREET TYPE: GREENWAYS

Character	Shared green pedestrian and cycle movement routes
Design speed	N/A

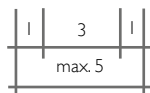
DESIGN ELEMENTS

Desired Radii	N/A
Trees	
Planting character	Responds to 4no. Landscape Character Areas
Lighting	✓
Bus route	N/A
On-street car parking	N/A
Traffic calming	N/A
Utilities	
Drainage channels	N/A

MATERIALS

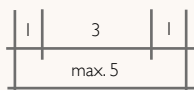
Materials palette	Informal treatment
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GREEN LINKS

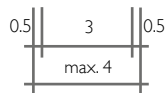


All measurements in metres

Green links provide designated pedestrian and/or cycle movement routes across Welborne. They typically connect open green spaces to the tertiary street network. The width of the green links shall vary but will provide a 3m wide pedestrian and cycle link. They will provide safe spaces, with lighting where appropriate and natural surveillance from neighbouring uses. The landscape design provides opportunities for tree planting, a range of natural habitats to increase biodiversity, places to meet and rest and, in some instances, larger areas that could accommodate local food growing or neighbourhood play spaces. The planting character will respond to the Landscape Character Area within which it is located.



OPEN SPACE ROUTES



All measurements in metres



Precedent example of an open space route

Open space routes run through the green spaces surrounding the development parcels and provide semi-natural and safe movement routes. They are divided hierarchically:

- Multi-user paths: footway/cycleway/bridleway catering for pedestrians, cyclists and horse riders; some are also combined with upgraded or diverted PRowWs.
- Footways and cycleways: Generally 3m-wide shared paths for pedestrians and cyclists.
- Footways: Formal paths up to 2.5m wide that provide desire lines through the development linking residential areas with the wider footpath network, open spaces and facilities.
- Tertiary footpath: Informal pathways up to 2m wide that provide a more rustic character to residential areas and/or a more convoluted, scenic route through open spaces.

Their design will ensure that:

- Surface finishes for each footway, footpath, cycleway and bridleway are appropriate for its location, purpose and frequency of use. Surfacing to be continuous such as self-binding aggregate or buff-coloured asphalt.
- Routes are of an accessible gradient, appropriately shaded and include regular resting stops.
- Lighting is provided in appropriate locations based on levels of usage; lighting design to consult with ecologists where necessary.
- Routes do not allow access to vehicles except for maintenance and access to infrastructure.
- Planting character responds to the Landscape Character Area it is located within.

Numerous cycle and/or pedestrian only accesses into the development will be provided or enhanced. Open space routes will incorporate signalised and informal crossing points, with the crossing design appropriate to its location and users.

7. STREET LAYOUT

7a. PRINCIPAL STREET TYPES

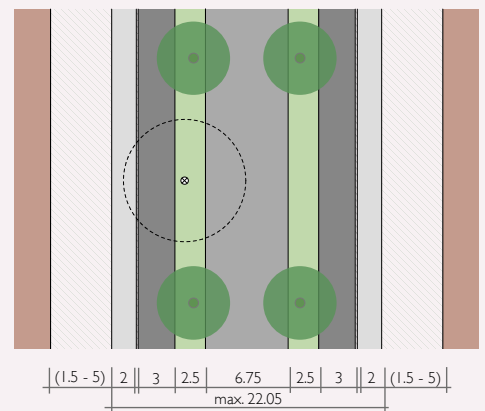
The patterns of movement that are laid out by streets often have a longevity far greater than the buildings that front onto the spaces. Hence great care shall be taken in the design and layout of the streets. This section sets out the key criteria for street design at Welborne. Each street form is described in terms of design criteria, typical dimensions, cross section, plan form and precedent examples.

The street network at Welborne comprises of typical streets as illustrated in the matrix of types opposite. In addition to these standard streets and paths are the bespoke conditions of Welborne Way and Knowle Road. These routes are two of the most important movement connections within Welborne.



WELBORNEWAY

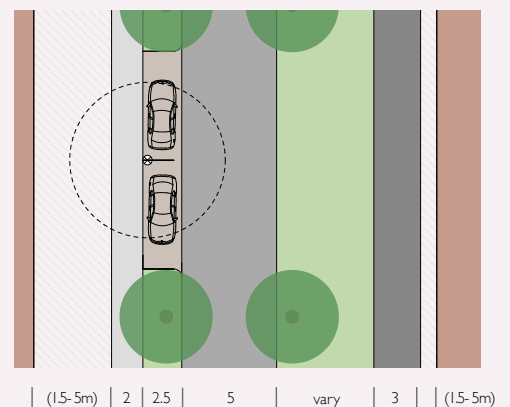
Welborne Way is the principal north-to-south movement route through Welborne, providing a connection to the M27. It shall be important to consider its role both as a vehicular movement route but also one that is conducive to pedestrian and cycle movements.



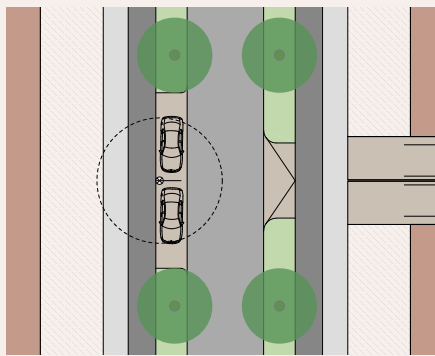
KNOWLE ROAD

Knowle Road is an existing 50mph road connecting Knowle village to the Wickham Road.

Knowle Road will take on a new character as a residential street with a reduced speed limit of 30mph.

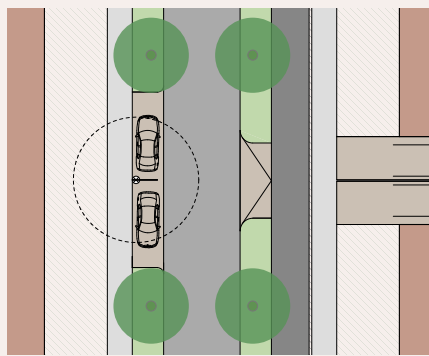


PRINCIPAL STREET TYPES USED ACROSS WELBORNE



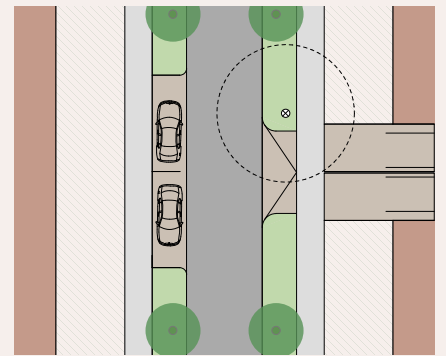
|(1.5 - 5)| 2 | 2 | 2.5 | 6.1 | 2.5 | 2 | |(1.5 - 5)|
max. 19.5

PRIMARY STREET



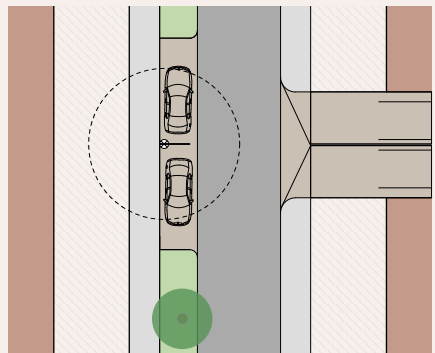
|(1.5 - 5)| 2 | 2.5 | (5.5 - *6.1) | 2.5 | 3 | 2 | |(1.5 - 5)|
max. 18.3

SECONDARY STREET
(*6.1 ONLY FOR BUS ROUTES)



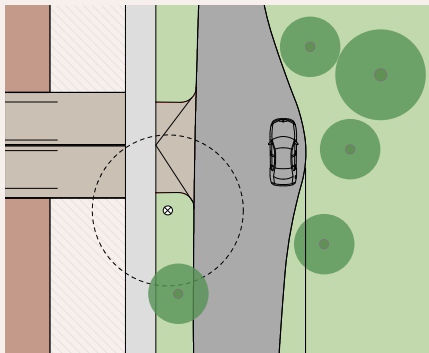
|(1.5 - 5)| 2 | 2.5 | (4 - 5.5) | 2.5 | 2 | |(1.5 - 5)|
max. 14.5

TERTIARY STREET - OPTION 1



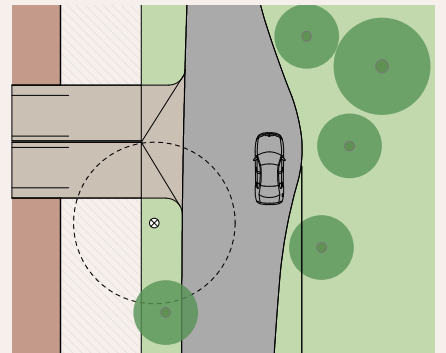
|(1.5 - 5)| 2 | 2.5 | 4 - 5.5 | 2 | |(1.5 - 5)|
max. 12

TERTIARY STREET - OPTION 2



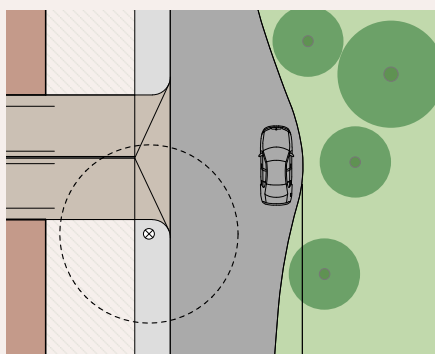
|(1.5 - 5)| 2 | (0.5 - 2.5) | (3.7 - 7.5)|
max. 12

EDGE LANE - OPTION 1



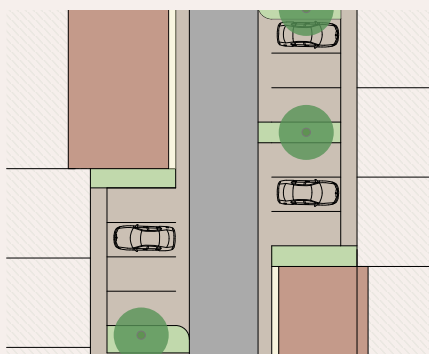
|(1.5 - 5)| (0.5 - 2.5) | (3.7 - 7.5)|
max. 10

EDGE LANE - OPTION 2



|(1.5 - 5)| 2 | (3.7 - 7.5)|
max. 12

EDGE LANE - OPTION 3



7.2 | 5 | 7.2 |
max. 19.4

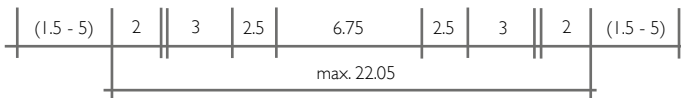
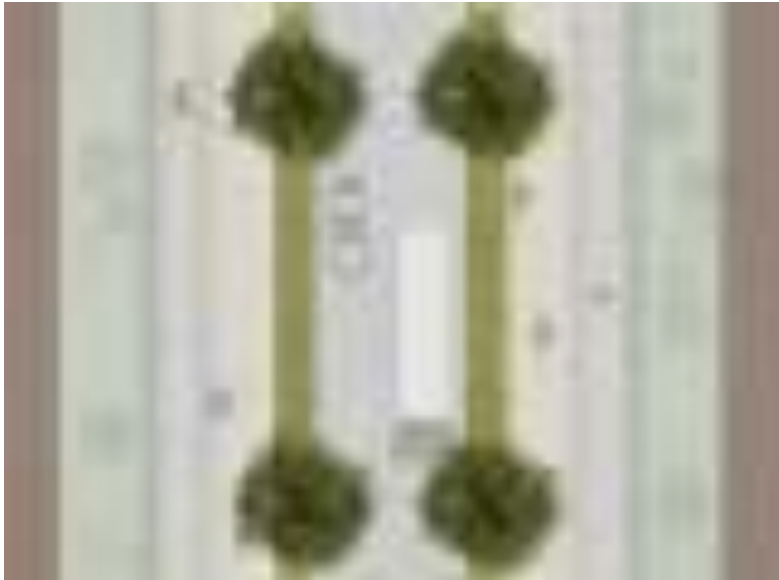
COURTYARD LANE

KEY

- Building block
- Privacy strip/front garden
- Cycleway
- Shared cycleway
- Footpath
- Tree verge
- Segregation strip
- Tree
- Carriageway
- Street lamp
- Parking

All measurements in metres

WELBORNE WAY

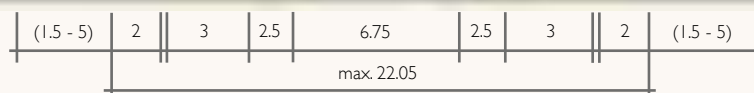


All measurements in metres

Welborne Way is the main north-south route through Welborne. Its characteristics will include:

- a generous carriageway with dedicated cycleways in both directions
- a tree verge and footway on both sides
- taller buildings between three- to five-storeys high fronting onto the road
- a tree verge and footway on both sides with tree and planting proposal compatible with SuDS system. Refer to Strategic Design Code for details.

Welborne Way will connect and pass through a number of the principal centres within the settlement including the Village Centre and District Centre. These are opportunities for 'special' junctions and places that emphasise the urban settings.





STREET TYPE: WELBORNE WAY

Character	Principal movement route, wider, busier, dedicated cycle facilities, taller buildings.
Design speed	30 mph

DESIGN ELEMENTS

Desired Radii	6.0m (vehicle tracking to be used)
Trees	In verge
Lighting	✓
Bus route	✓
On-street car parking	None
Traffic calming	Junction spacing, squares, bends, positioning of buildings and trees, activity
Utilities	Under footway
Drainage channels	At edge

MATERIALS

Materials palette	Typical treatment
-------------------	-------------------

WELBORNE WAY

LINK CAPACITY

A VISSIM micro simulation model has been prepared to provide data on the traffic implications within the internal Welborne highway network, specifically Welborne Way.

The coding of the VISSIM model has considered the required street width of Welborne Way of 6.75m. Outputs from the model have confirmed that users of Welborne Way would not experience material delays and free flow of traffic would not be compromised.

Examples of the average speeds of traffic using Welborne Way during peak times have been identified using 'Heat Maps' extracted from the VISSIM model. Yellow sections of the Heat Map indicate average vehicle speeds of 30 mph. Given the absence of significant junctions formed with Welborne Way, the majority of Welborne Way will see average traffic speeds of 30 mph being achieved, confirming that sufficient link capacity will be available.

FRONTAGE ACTIVITY

There will be frontages along Welborne Way to provide a high-quality urban environment with a strong sense of place. The frontages will provide good passive surveillance and encourage active travel along the length of Welborne Way. Frontages will mostly be residential. However, there will be distinct areas of leisure and commercial units, in areas close to the Village Centre to the north of Welborne Way, and the District Centre to the south. No further new access points will be facilitated from these centres onto Welborne Way. The centres will be serviced either through access points currently proposed on Welborne Way or by the wider street network.

However, there will be minimal direct accesses onto Welborne Way, as this would compromise the traffic capacity and lead to interruptions of the proposed footway and cycleways.

PARKING RESTRICTIONS

Double yellow lines (Dia 1018.1) will restrict on-street parking along Welborne Way. The gap between the edge of the carriageway and the nearest edge of the longitudinal line shall be approximately 250mm. The lines will be 50mm wide, with a 50mm gap between the lines; they will be painted in No 353 (Deep Cream).



Right: No 353 (Deep Cream)

JUNCTION LOCATIONS AND DESIGN

The main junction on Welborne Way is at the intersection with Dashwood Avenue.

Other junctions along the route shall be priority junctions with passing bays per CD123 Figure 6.25N2.

See Section 7 for diagrams of typical junctions on Welborne Way.

MINIMUM CENTRELINE CURVE RADII

The design speed for Welborne Way will be 30 mph/48 kph; the minimum centreline curve radii along Welborne Way will be 41m, as per Table 8.1 of Manual for Streets 2.

FOOTWAY AND CYCLEWAY DETAILS

To encourage active transport, Welborne Way will have a segregated two-way cycle and pedestrian route on both sides of the carriageway. A 150mm channel block will provide segregation between the pedestrian and cycle routes.



Above: Visualisation of Welborne Way

Segregated routes will be provided in preference to shared space routes as they are safer for visually impaired users and young children; they also provide better facilities for cyclists.

Cycleways will be 3m wide, to accommodate two-way cycle traffic, recumbents and child's trailers.

Footways will be 2m wide to allow buggies or wheelchairs to pass each other.

PUBLIC TRANSPORT PROVISION

There will be a proposed bus route along Welborne Way. To avoid interrupting traffic flows, bus stops shall be provided in lay-bys. The Bus Rapid Transit (BRT) route will run along Welborne Way. However, no specific BRT infrastructure will be required to facilitate this, as the route provides sufficient traffic capacity and queuing is not expected.

WELBORNE WAY

PHASING AND DELIVERY

Welborne Way will be delivered in 2 phases

Phase 1 - Welborne Way (Knowle Road to Broadway) Works - the works consisting of the provision of a vehicular link between Knowle Road and Broadway (to include the construction of Welborne Way and the construction of a junction at Knowle Road and the construction of the vehicular link to Broadway) as shown in principle on plan 7328_104 Rev D and as agreed in accordance with any Highways Agreement;

Phase 2 - A32/Welborne Way Roundabout Works
- the provision of a permanent vehicular roundabout

access onto the A32 as shown in principle on the plan 6091/GA/311 Rev G including the shared use route extension on the southern arm to the "A32 / Knowle Road / Chalk Lane Roundabout Works and as agreed in accordance with any Highways Agreement.

It is important that these junctions are delivered in a timely manner as their main purpose is to alleviate congestion and delays on the A32. Although the transport assessment demonstrated the Welborne Way is only required prior to the occupation of 1,851 units and the roundabout by 2,601 units Buckland, as master developer, will monitor the capacity of Knowle Road and Broadway Roundabout annually from the

Phase 1 - Welborne Way (Knowle Road to Broadway) Works



occupation of 1,160 units. In the event that the RFC values exceed 0.75 on any one arm of the Knowle Road and Broadway Roundabouts, WLL will complete the construction of Welborne Way between Broadway Roundabout to the south and Knowle Road to the north within 12 months. If the RFC values do not exceed 0.75 Buckland will complete the road prior to

- Occupation of 1851 Residential Units within Area X; or
- Occupation of any Residential Unit or Commercial Unit within Area Y; or
- More than 25% Occupation of any land use with the Employment Area;

Once Welborne Way is open to traffic Buckland will continue to monitor Knowle Road and Broadway Roundabout, in the event the RFC values exceed 0.75 on any one arm of the Knowle Road and Broadway Roundabouts, WLL will commence the construction of Welborne Way Roundabout within 6 months. In order to safeguard the earlier delivery of the roundabout, if required, Buckland will submit a S278 preliminary design check prior to the occupation of the 700th units. If the RFC values do not exceed 0.75 Welborne Way Roundabout will be complete prior to the occupation of 2,601st unit

Phase 2 - A32/Welborne Way Roundabout Works

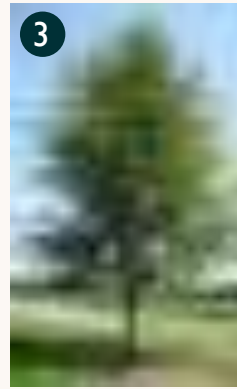
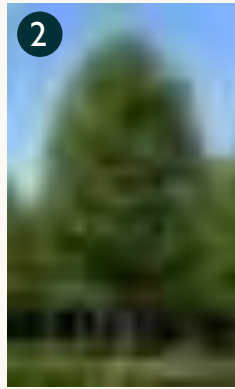


WELBORNE WAY: PLANTING

Effect / Character	<ul style="list-style-type: none"> • Formal avenue tree planting with native grassland rich in local wildflora • Continous, consistent character through three Landscape Character Areas
Arrangement	<ul style="list-style-type: none"> • Trees located centre to the verge strip at regular intervals • Specimen trees placed at junctions or in strategic locations shall enhance and frame views • Native species-rich grassland beneath • Street light columns distance to be min. 5m from tree stem. Refer to lighting column guidance • Tree planting to provide a continuous canopy effect and enhance both pedestrain and cyclist experience
Tree type	<ul style="list-style-type: none"> • Trees to have neat, conical crown canopy shape • Large tree species over 10m • Tree species compatible with SuDS system where located within swales • Semi-mature single clear-stem species (min 2.1m) to avoid visibility issues and clashes with vehicles • Seasonal interest trees as landmak trees are supported in association with footway junctions and resting places
Verge Types	<ul style="list-style-type: none"> • Species-rich grassland with diverse native wildflora to provide functional biodiversity gain and aesthetic benefit for street character and quality. The diverse grassland overlies a permeable tree planting zone as a continuous trench that will support healthy establishment and long-term growth. Careful placement of root directing barrier will protect below-ground utilities. Surface water will attenuate via the root zone by the sensitively integrated drainage design, enabling vegetation to support silt and nutrient level reduction. from surface water run-off. The cutting regime will maintain a high-quality appearance, for higher infiltration rates and to encourage species diversity



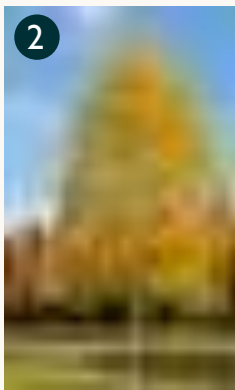
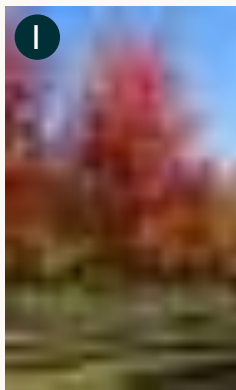
INDICATIVE STREET TREES



1. *Quercus palustris*
2. *Platanus x hispanica*
3. *Fagus sylvatica*

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
<i>Quercus palustris</i> (Pin Oak, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
<i>Platanus x hispanica</i> (London Plane, Large)	35-45	
<i>Fagus sylvatica</i> (Beech, Large)	35-45	

INDICATIVE LANDMARK TREES



1. *Liquidambar styraciflua*
2. *Liriodendron tulipifera*
3. *Quercus robur*

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
<i>Liquidambar styraciflua</i> (Sweet Gum, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
<i>Liriodendron tulipifera</i> (Tulip Tree, Large)	35-45	
<i>Quercus robur</i> (Pedunculate Oak, Large)	35-45	

INDICATIVE VERGE GRASSLAND

SPECIES MAY INCLUDE
A carefully selected combination of finer, low-growing grass species as an open matrix for wildflora:
Birds-foot-trefoil, Buttercup
Clover
Cowslip
Eyebright
Knapweed
Lawn Chamomile
Ragged Robin
Selfheal
Yarrow

Species selection to respond to Landscape Character Area

KNOWLE ROAD



Knowle Road is a one-off secondary street. It is based on the alignment of the existing road, with some realignment. The existing 50mph speed limit will be lowered. It retains existing planting where possible and introduces additional tree planting. Planting should celebrate the Woodland Landscape Character Area.

Verge planting includes existing grassland planting where retention is possible on the southern verge with lawn and meadow planting on the northern verge.



Note: This is an existing shared cycle/pedestrian route

All measurements in metres





STREET TYPE: KNOWLE ROAD

Character	Woodland and Downland Landscape Character Areas
Design speed	Exiting 50mph to be lowered

DESIGN ELEMENTS

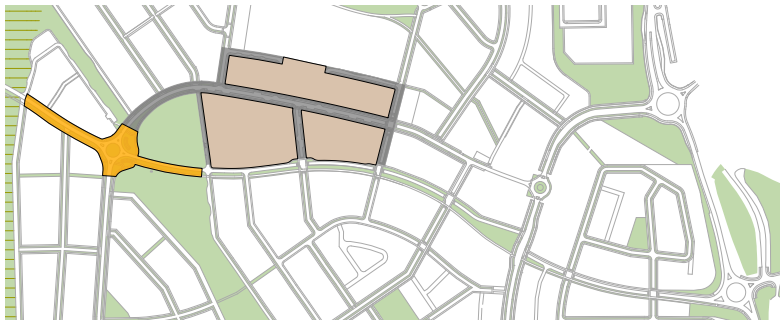
Desired Radii	6.0m
Trees	In verge
Planting character	Bespoke character incorporating existing planting. Planting responds to Landscape Character Area
Lighting	✓
Bus route	N/A
On-street car parking	Parallel
Traffic calming	Junction spacing, squares, positioning of buildings and trees, activity
Utilities	Existing and proposed under footway and cycle route
Drainage channels	At edge

MATERIALS

Materials palette	Typical treatment
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KNOWLE ROAD

PHASING AND DELIVERY



Phase 1 – Construction works to Knowle Road from the western site boundary to Welborne Park Roundabout. This section of Knowle Road to remain 6.7m in width.



Phase 2 – Construction works to Knowle Road from Welborne Park to Welborne Way. Road width reduced to 5.5m. 2a delivered with adjacent residential units 2b delivered with Village Centre.



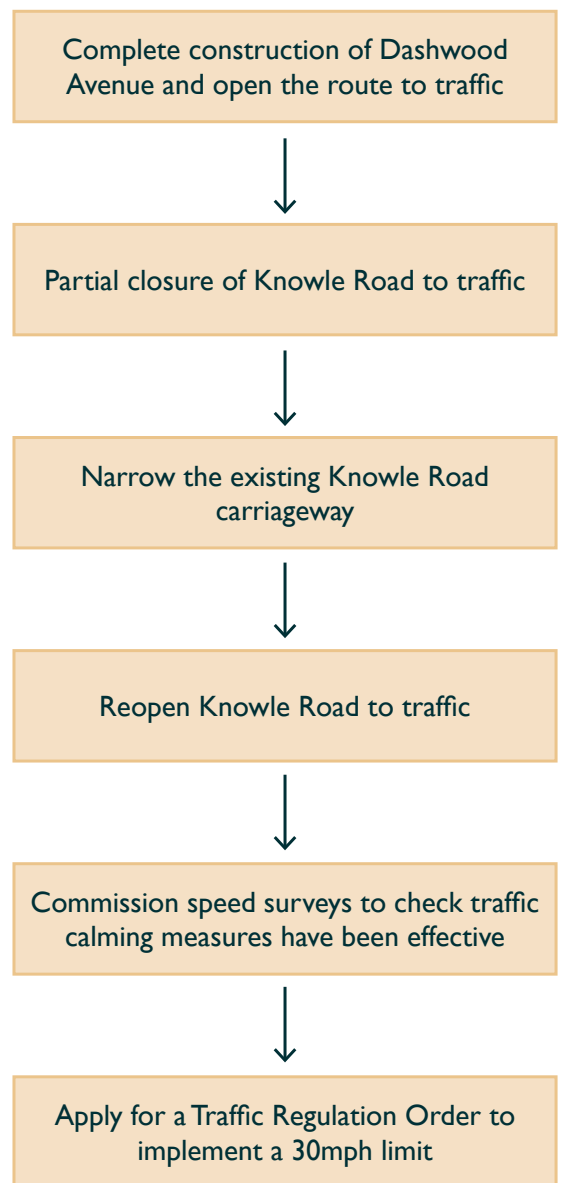
Phase 3 – Construction works to Knowle Road from Welborne Way to A32. Road width reduced to 5.5m.

SPED CONTROL CAPACITY

To reduce the travelling speed along Knowle Road it shall be narrowed to 5.5m.

The provision of the raised junctions will slow traffic.

Where Knowle Road crosses Welborne Way, the junctions will be staggered to reduce conflict at the crossing. The cycleway and footpath shall remain on the current and most direct alignment.



KNOWLE ROAD: PLANTING



1. Northern verge: Formal tree spacings, Medium street trees with landmark trees at junctions or ends of parking bays. Use of smaller ornamental multistem trees and hedge species within understory verge
2. Southern verge: Informal spacings, individual trees, clusters and groups, retaining existing trees and shrub planting where possible, inclusion of rich understory of hedge/shrub species and flowering and ornamental multistems
3. Existing planting retained where possible
4. Additional tree planting within retained grassland area
5. Proposed verge and planting to respond to Woodland Landscape Character Area
6. Hedgerows species to respond to Woodland Landscape Character Area

INDICATIVE TREES

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
Northern verge: Street trees		
Acer campestre (Field Maple, Medium)	30 - 40	26m ³ of Stockholm System, which is equivalent to 20m ³ of Topsoil
Alnus cordata (Italian Alder, Medium)	30 - 40	
Ulmus 'New Horizon' (resistant Elm, Medium)	30 - 40	
Northern verge: Landmark and ornamental street trees		
Juglans regia (Walnut, Large)	30-40	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
Quercus robur (Pedunculate Oak, Large)	30-40	
Tilia cordata (Lime, small-leaved, Large)	30-40	
Southern verge: Street trees		
Existing retained trees and shrubs		
Acer campestre (Field Maple)		
Prunus Avium (Wild Cherry)		
Corylus avellana (Hazel)		
Quercus robur (Oak)		

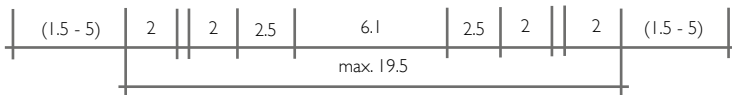
PRIMARY STREETS



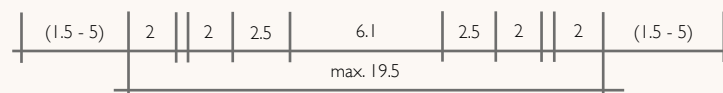
Primary streets provide the main green infrastructure within the masterplan. They are the widest streets, with footways and tree verges on either side and taller buildings than on other routes.

These streets are planted with large single tree species selected for their resilience and provision of important habitat. Large canopies will soften and integrate the development within the landscape.

Primary streets will include one way cycle lanes on separate sides of the street.



All measurements in metres





STREET TYPE: PRIMARY STREETS

Character	More formal design, constant cross section, larger trees, important routes
Design speed	20 mph

DESIGN ELEMENTS

Desired Radii	6.0m (vehicle tracking to be used)
Trees	In verge
Lighting	✓
Bus route	N/A
On-street car parking	Parallel or chevron (unmarked)
Traffic calming	Junction spacing, squares, bends, positioning of buildings and trees, activity
Utilities	Under footway
Drainage channels	At edge

MATERIALS

Materials palette	Typical treatment
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PRIMARY STREETS

Effect / Character	<ul style="list-style-type: none"> • Formal tree planting with verge planting • Continous consistent character through 4no. landscape character areas
Arrangement	<ul style="list-style-type: none"> • Trees located centre to the verge strip at regular intervals • Single species throughout • Specimen trees placed at junctions or in strategic locations shall enhance and frame views
Tree type	<ul style="list-style-type: none"> • Tall, large tree species over 10m • Trees to have neat, conical crown canopy shape • Single clear-stem species are supported to avoid visibility issues • Semi-mature types are encouraged to balance with the height of proposed buildings along this street type • Flowering and fruiting trees as landmark trees are supported in association with secondary street junctions • Tree planting to avoid fruiting tree species if associated with on-street parking underneath
Verge Type	<ul style="list-style-type: none"> • Species-rich grassland with diverse native wildflora to provide functional biodiversity gain and aesthetic benefit for street character and quality. The diverse grassland overlies a permeable tree planting zone as a continuous trench that will support healthy establishment and long-term growth. Careful placement of root directing barrier will protect below-ground utilities. Surface water will attenuate via the root zone by the sensitively integrated drainage design, enabling vegetation to support silt and nutrient level reduction. from surface water run-off. The cutting regime will maintain a high-quality appearance, for higher infiltration rates and to encourage species diversity



- 1. Tree planting
- 2. Verge grassland
- 3. Private hedgerows

INDICATIVE STREET TREES



1. *Carpinus betulus*
2. *Corylus colurna*
3. *Fagus sylvatica*
4. *Tilia cordata*

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
<i>Carpinus betulus</i> (Hornbeam, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
<i>Corylus colurna</i> (Turkish Hazel, Large)	35-45	
<i>Fagus sylvatica</i> (Beech, Large)	35-45	
<i>Tilia cordata</i> (Small Leaf Lime, Large)	35-45	

INDICATIVE LANDMARK TREES



1. *Juglans regia*
2. *Liriodendron tulipifera*
3. *Pinus sylvestris*

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
<i>Juglans regia</i> (Walnut, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
<i>Liriodendron tulipifera</i> (Tulip Tree, Large)	35-45	
<i>Pinus sylvestris</i> (Scots Pine, Large)	35-45	

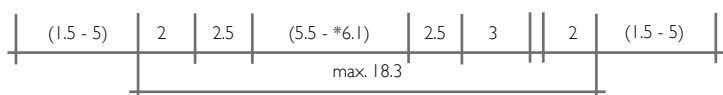
Species selection to respond to Landscape Character Area

SECONDARY STREETS

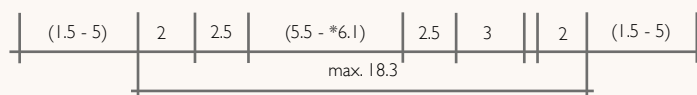


Secondary streets are the arteries that link the busier primary street network to the heart of each neighbourhood, providing key routes for the tertiary streets and edge lanes to connect into.

Planting will reflect and celebrate the Landscape Character Areas within which the secondary street is located and the existing soil conditions. Verge planting will be predominantly lawn with the option of meadow planting and highlight planting that may be used to enhance spaces such as key junctions, public buildings and school entrances.



All measurements in metres. *6.1 only for bus routes





STREET TYPE: SECONDARY STREETS

Character	Human scale, tree lined, low traffic volume and speed residential areas.
Design speed	20 mph

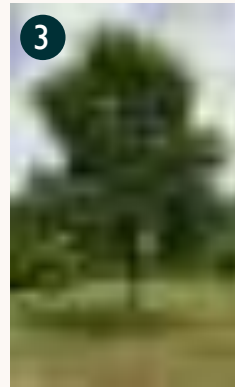
DESIGN ELEMENTS

Desired Radii	2.0m (vehicle tracking to be used)
Trees	In verge
Planting character	Responds to 4no. Landscape Character Areas.
Lighting	✓
Bus route	N/A
On-street car parking	Parallel (unmarked)
Traffic calming	Junction spacing, squares, bends, positioning of buildings and trees, activity
Utilities	Under footway
Drainage channels	At edge

MATERIALS

Materials palette	Typical treatment
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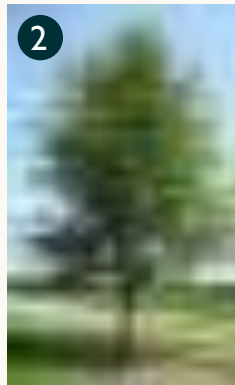
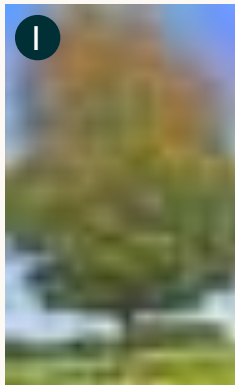
INDICATIVE
STREET TREES
ALONGSIDE
FOOTWAYS



1. *Acer campestre*
2. *Alnus cordata*
3. *Ulmus 'New Horizon'*

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
<i>Acer campestre</i> (Field Maple, Medium)	30-40	26m ³ of Stockholm System, which is equivalent to 20m ³ of Topsoil
<i>Alnus cordata</i> (Italian Alder, Medium)	30-40	
<i>Ulmus 'New Horizon'</i> (Resistance Elm, Medium)	30-40	

INDICATIVE
STREET TREES
ALONGSIDE
CYCLEWAYS

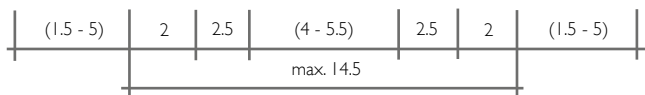


1. *Carpinus betulus*
2. *Fagus sylvatica*
3. *Tilia cordata*

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
<i>Carpinus betulus</i> (Hornbeam, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
<i>Fagus sylvatica</i> (Beech, Large)	35-45	
<i>Tilia cordata</i> (Small Leaf Lime, Large)	35-45	

Species selection to respond to Landscape Character Area

TERTIARY STREETS 1

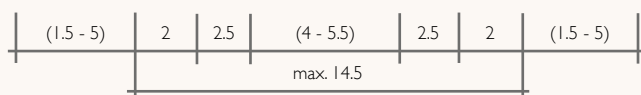


All measurements in metres

Tertiary streets are the most common street type at Welborne and will primarily be low-trafficked residential streets. The carriage way width can vary, depending on the status of the street and intended character.

Access to and from houses plays an important role in the design of tertiary streets. The design needs to both establish a sense of privacy and sense of community. Their ability to contribute to placemaking will be crucial to the development's success.

Tertiary streets will use similar planting types as secondary streets to maintain visual consistency and enhance Landscape Character Areas. They will use a wider variety of smaller tree species, particularly flowering, fruiting and nut-bearing varieties of benefit to wildlife. Planted verges provide an opportunity to maintain the distinctiveness of each Landscape Character Area via wild flower and grass mixtures. Variation in planting treatment for private front gardens shall also reflect Landscape Character Areas. Species will complement fence systems or retaining walls. Where used as private boundary treatments, hedgerows will respond to visibility and safety requirements when associated with access and egress to private dwellings.





STREET TYPE: TERTIARY STREETS

Character	Human scale, tree lined, low traffic volume and speed residential areas
Design speed	20 mph

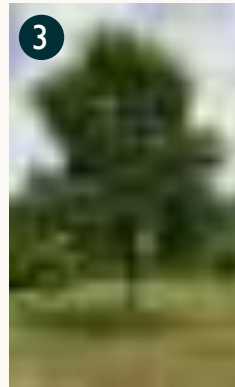
DESIGN ELEMENTS

Desired Radii	2.0m (vehicle tracking to be used)
Trees	In verge
Planting character	Responds to 4no. Landscape Character Areas
Lighting	✓
Bus route	N/A
On-street car parking	Parallel, intermittent within verge (unmarked)
Traffic calming	Junction spacing, squares, bends, positioning of buildings and trees, activity
Utilities	Under footway
Drainage channels	At edge

MATERIALS

Materials palette	Typical treatment
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INDICATIVE STREET TREES



1. Acer campestre
2. Alnus cordata
3. Ulmus 'New Horizon'

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
Acer campestre (Field Maple, Medium)	30-40	26m ³ of Stockholm System, which is equivalent to 20m ³ of Topsoil
Alnus cordata (Italian Alder, Medium)	30-40	
Ulmus 'New Horizon' (Resistance Elm, Medium)	30-40	

INDICATIVE LANDMARK TREES

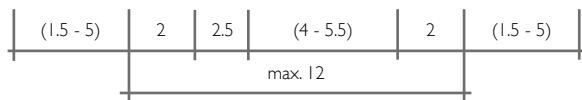


1. Juglans nigra
2. Maytenus boaria

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
Juglans nigra (Black Walnut, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
Maytenus boaria (Mayten, Large)	35-45	

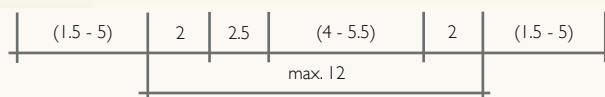
Species selection to respond to Landscape Character Area

TERTIARY STREETS 2



Tertiary streets 2 are a variant of the tertiary street type that has a grassed verge to only one side of the street, with a single line of tree planting. It shall be used for lower-status streets across Welborne.

All measurements in metres





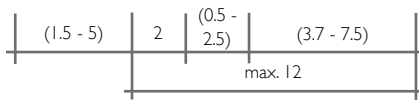
Example of tertiary street 2 type

EDGE LANES 1

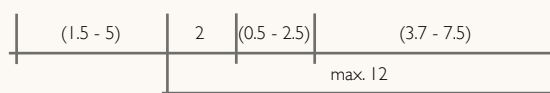


Edge lanes are found on the edges of the Garden Village. They are the interface between the development and open spaces, such as along the northwestern edges facing the Welborne Mile or Dashwood. For these types of road, there is likely to be:

- A carriageway capable of accommodating a single lane of traffic in either direction with a footway on one side
- Homes and private front gardens fronting on to the carriageway on both sides, or on just one side with open space on the other side



All measurements in metres





STREET TYPE: EDGE LANES

Character	Shared routes for all modes on green edges of Welborne. Low speed, variable width, level surface, informal parking.
Design speed	10 mph

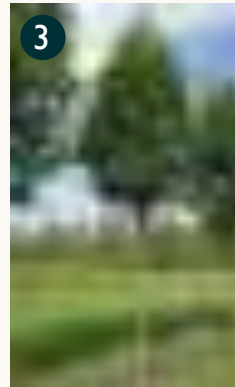
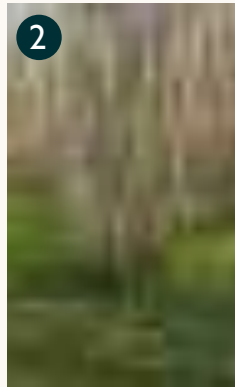
DESIGN ELEMENTS

Desired Radii	N/A
Trees	At edges
Planting character	Responds to 4no. Landscape Character Areas
Lighting	To be agreed based on location and Landscape Character Area
Bus route	N/A
On-street car parking	Parallel, informal (unmarked)
Traffic calming	Narrowing to 3.7m, car parking
Utilities	In adjacent green space
Drainage channels	At edge

MATERIALS

Materials palette	Typical treatment
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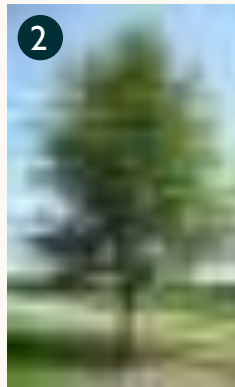
INDICATIVE STREET TREES ALONGSIDE FOOTWAYS



1. *Acer campestre*
2. *Corylus avellana*
3. *Sorbus torminalis*

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
<i>Acer campestre</i> (Field Maple, Medium)	30-40	26m ³ of Stockholm System, which is equivalent to 20m ³ of Topsoil
<i>Corylus avellana</i> (Hazel Coppice, Medium)	30-40	
<i>Sorbus torminalis</i> (Wild Service Tree, Medium)	30-40	

INDICATIVE STREET TREES ALONGSIDE CYCLEWAYS

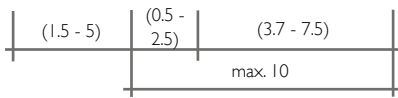


1. *Quercus robur*
2. *Fagus sylvatica*
3. *Tilia cordata*

SPECIES MAY INCLUDE	GIRTH (cm)	SOIL VOLUME (m ³ min)
<i>Quercus robur</i> (Pedunculate Oak, Large)	35-45	36m ³ of Stockholm System, which is equivalent to 28m ³ of Topsoil
<i>Fagus sylvatica</i> (Beech, Large)	35-45	
<i>Tilia cordata</i> (Small Leaf Lime, Large)	35-45	

Species selection to respond to Landscape Character Area

EDGE LANES 2



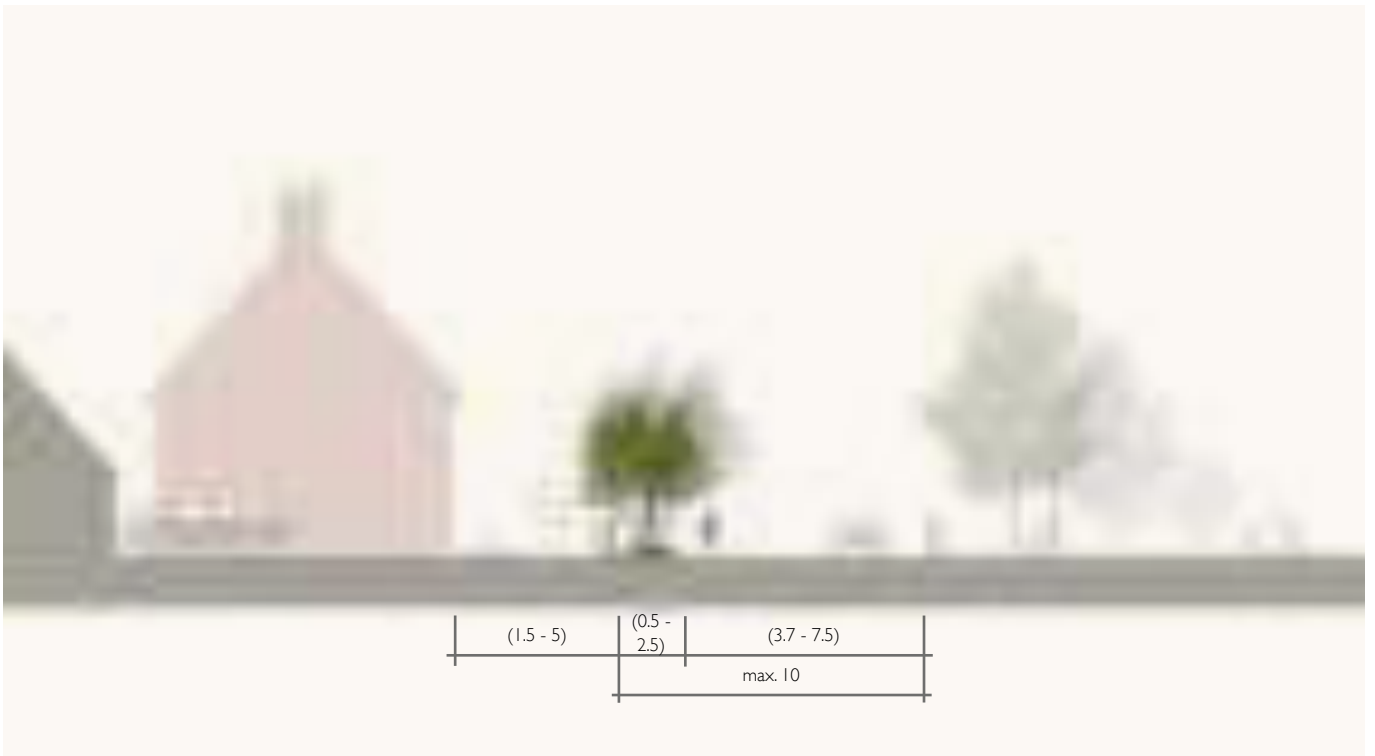
All measurements in metres

The planting character of edge lanes should respond to the Landscape Character Area it is located within and the open space it is fronting.

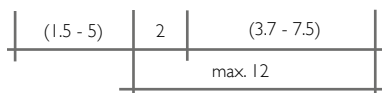
Tree verges may be combined with parking spaces. Trees within verges may vary between single to multi-stem, and will use smaller species corresponding to the Landscape Character Area.

Trees may be planted at irregular intervals to offer an informal, looser character. (For trees within open space refer to the Strategic Design Code.)

Boundary treatments vary between Landscape Character Areas. These may be hedgerows, fencing or earth mounding. They should be appropriate for the anticipated use of the adjacent open space.



EDGE LANES 3

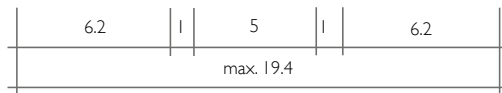


All measurements in metres





COURTYARD LANES

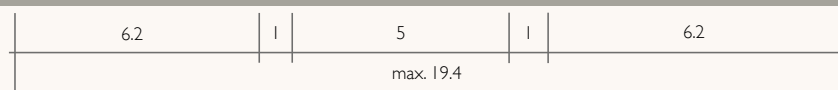


All measurements in metres

Courtyard lanes provide a secondary network of movement routes. They are shared surface streets providing vehicular and parking access to the rear of properties. The design should prioritise pedestrian movement.

Courtyard lanes shall be used by refuse vehicles for collections and so the lanes need to be designed and vehicle tracked to accommodate this.

It shall be important that sufficient space is allowed for the inclusion of trees and planting areas. This shall be used to break up runs of parking and rear boundaries and to make the lanes enjoyable routes for pedestrians and cyclists.





STREET TYPE: COURTYARD LANES

Character	Informal combining residential parking and pedestrian and cycle movement routes.
Design speed	20 mph

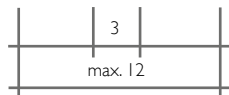
DESIGN ELEMENTS

Desired Radii	6.0m (vehicle tracking to be used)
Trees	Between parking spaces, adjacent to boundary walls
Planting character	Responds to 4no. Landscape Character Areas
Lighting	✓
Bus route	N/A
On-street car parking	Allocated private parking with some visitor parking
Traffic calming	Bends, narrowings, positioning of buildings, trees, activity
Utilities	In designated service margin
Drainage channels	At edge or centrally

MATERIALS

Materials palette	Typical treatment
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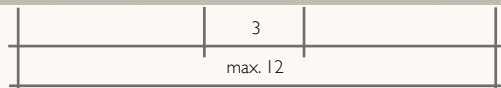
GREENWAYS



All measurements in metres

Greenways provide multifunctional, continuous green routes through the development. They are a key part of the green infrastructure that:

- Provide site-wide strategic east–west non-vehicle infrastructure for pedestrians, cyclists and horse riders
- Incorporate existing, diverted or upgraded Public Right of Ways
- Contain medium–large tree planting to break up the visual mass of the development when viewed from the south
- Generally have development either side
- Incorporate neighbourhood play and fitness trails, as per the Play Strategy
- Provide a range of natural habitats and continuous corridors for wildlife





- Vary in width to accommodate uses and facilities (min. 12m)
- Prioritise safety, through lighting (where appropriate) and natural surveillance from neighbouring uses
- Be easily accessed at regular intervals without barriers
- Provide resting points

The planting character should respond to the Landscape Character Area it is located within, with a focus on native and natural planting. To achieve this, planting may include species identified in the Strategic Design Code planting appendix.

STREET TYPE: GREENWAYS

Character	Shared green pedestrian and cycle movement routes
Design speed	N/A

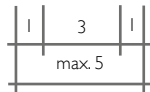
DESIGN ELEMENTS

Desired Radii	N/A
Trees	
Planting character	Responds to 4no. Landscape Character Areas
Lighting	✓
Bus route	N/A
On-street car parking	N/A
Traffic calming	N/A
Utilities	
Drainage channels	N/A

MATERIALS

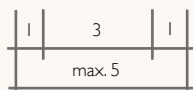
Materials palette	Informal treatment
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GREEN LINKS

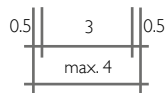


All measurements in metres

Green links provide designated pedestrian and/or cycle movement routes across Welborne. They typically connect open green spaces to the tertiary street network. The width of the green links shall vary but will provide a 3m wide pedestrian and cycle link. They will provide safe spaces, with lighting where appropriate and natural surveillance from neighbouring uses. The landscape design provides opportunities for tree planting, a range of natural habitats to increase biodiversity, places to meet and rest and, in some instances, larger areas that could accommodate local food growing or neighbourhood play spaces. The planting character will respond to the Landscape Character Area within which it is located.



OPEN SPACE ROUTES



All measurements in metres



Precedent example of an open space route

Open space routes run through the green spaces surrounding the development parcels and provide semi-natural and safe movement routes. They are divided hierarchically:

- Multi-user paths: footway/cycleway/bridleway catering for pedestrians, cyclists and horse riders; some are also combined with upgraded or diverted PRowWs.
- Footways and cycleways: Generally 3m-wide shared paths for pedestrians and cyclists.
- Footways: Formal paths up to 2.5m wide that provide desire lines through the development linking residential areas with the wider footpath network, open spaces and facilities.
- Tertiary footpath: Informal pathways up to 2m wide that provide a more rustic character to residential areas and/or a more convoluted, scenic route through open spaces.

Their design will ensure that:

- Surface finishes for each footway, footpath, cycleway and bridleway are appropriate for its location, purpose and frequency of use. Surfacing to be continuous such as self-binding aggregate or buff-coloured asphalt.
- Routes are of an accessible gradient, appropriately shaded and include regular resting stops.
- Lighting is provided in appropriate locations based on levels of usage; lighting design to consult with ecologists where necessary.
- Routes do not allow access to vehicles except for maintenance and access to infrastructure.
- Planting character responds to the Landscape Character Area it is located within.

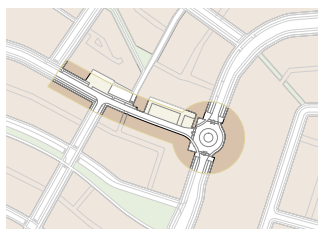
Numerous cycle and/or pedestrian only accesses into the development will be provided or enhanced. Open space routes will incorporate signalised and informal crossing points, with the crossing design appropriate to its location and users.

7b. KEY JUNCTIONS & SPECIAL PLACES

The following junctions and special places have been identified as locations that will require a non-standard street and junction design solution.

The detailed coding regarding the design of these spaces shall be provided in the relevant Neighbourhood Design Codes.

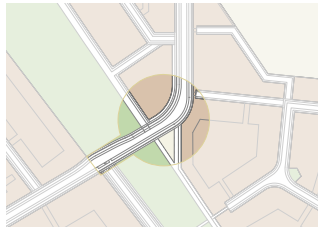
1



Village Centre

Linking to Welborne Way, the Village Centre shall be a focal square with commercial buildings, parking and specimen tree planting. The junction between the primary road network and Welborne Way will be incorporated as part of the public space design.

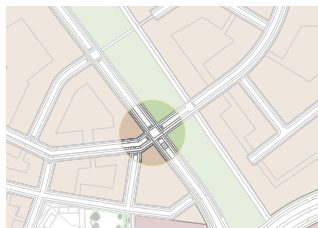
2



District Centre

This shall be the principal civic space within the settlement. Welborne Way will pass through the space. The design shall give priority to pedestrian movement by reducing vehicle speeds but with maintained traffic flow.

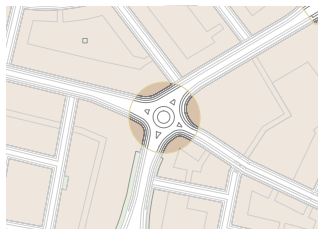
3



Welborne Park South

A key junction where a primary street and secondary street meet adjacent to Welborne Park. It will be designed to prioritise pedestrian east-to-west movement across the park.

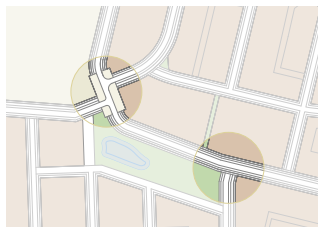
4



Primary Road Network South

A proposed roundabout junction that connects the primary road network with the M27 link. It will be designed as a special roundabout feature with non-standard geometry.

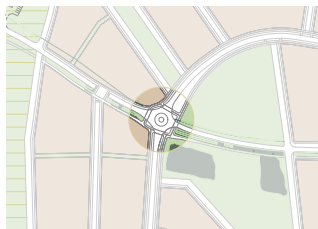
5



Primary Road Network South West

Two junctions of secondary streets adjoining the primary road network that shall require non-standard solutions.

6



Knowle Road West

The junction where the primary road network meets Knowle Road adjacent to the northern end of Welborne Park.

KEY JUNCTIONS PLAN

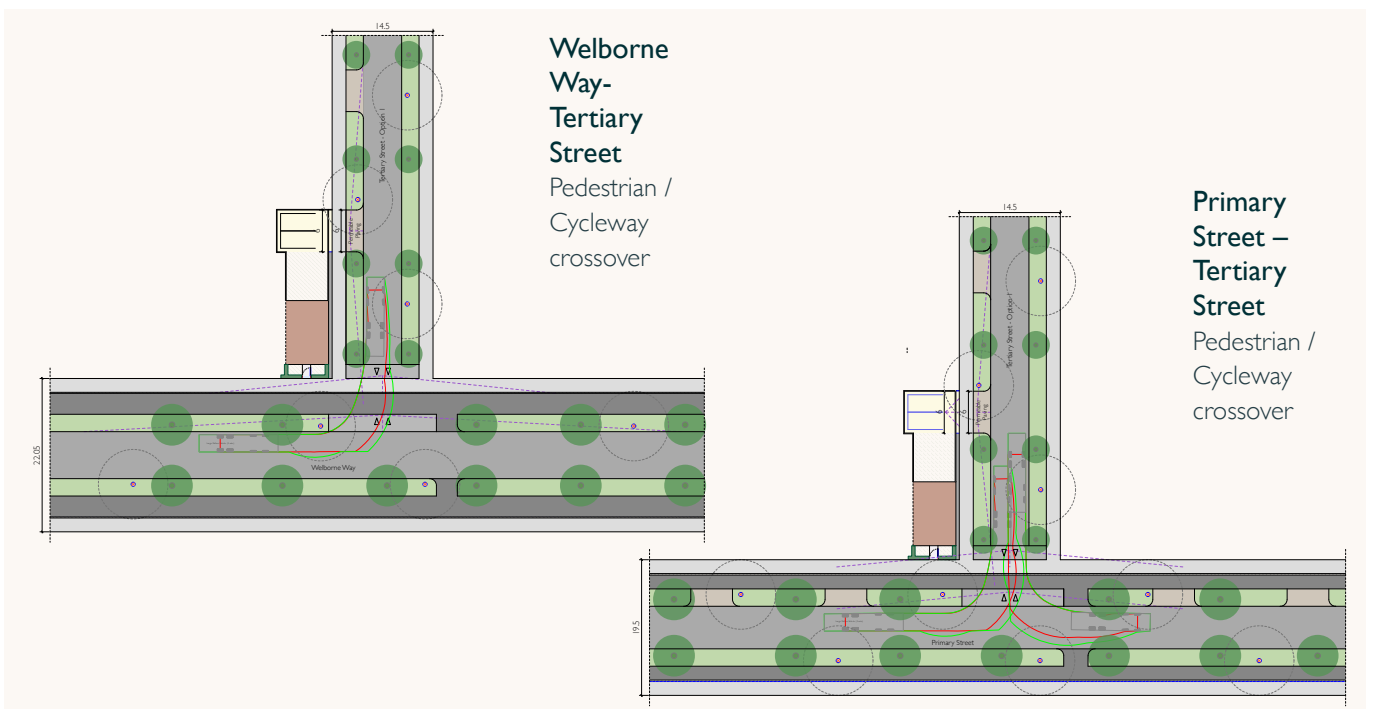


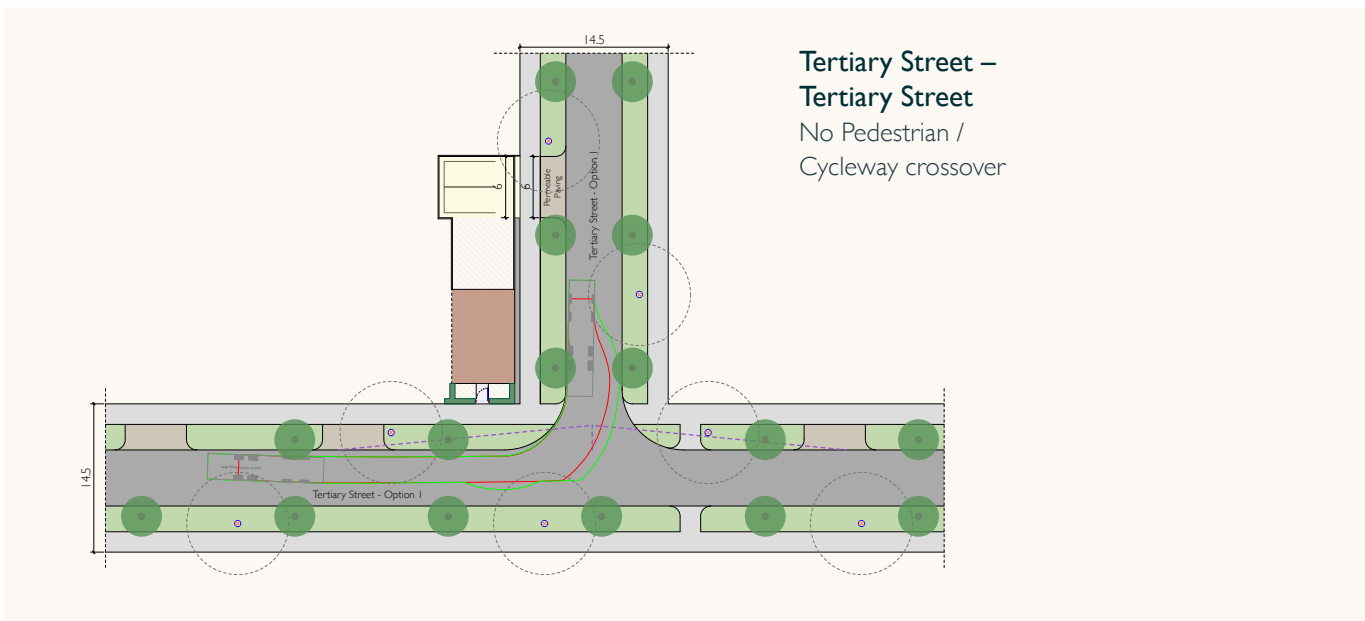
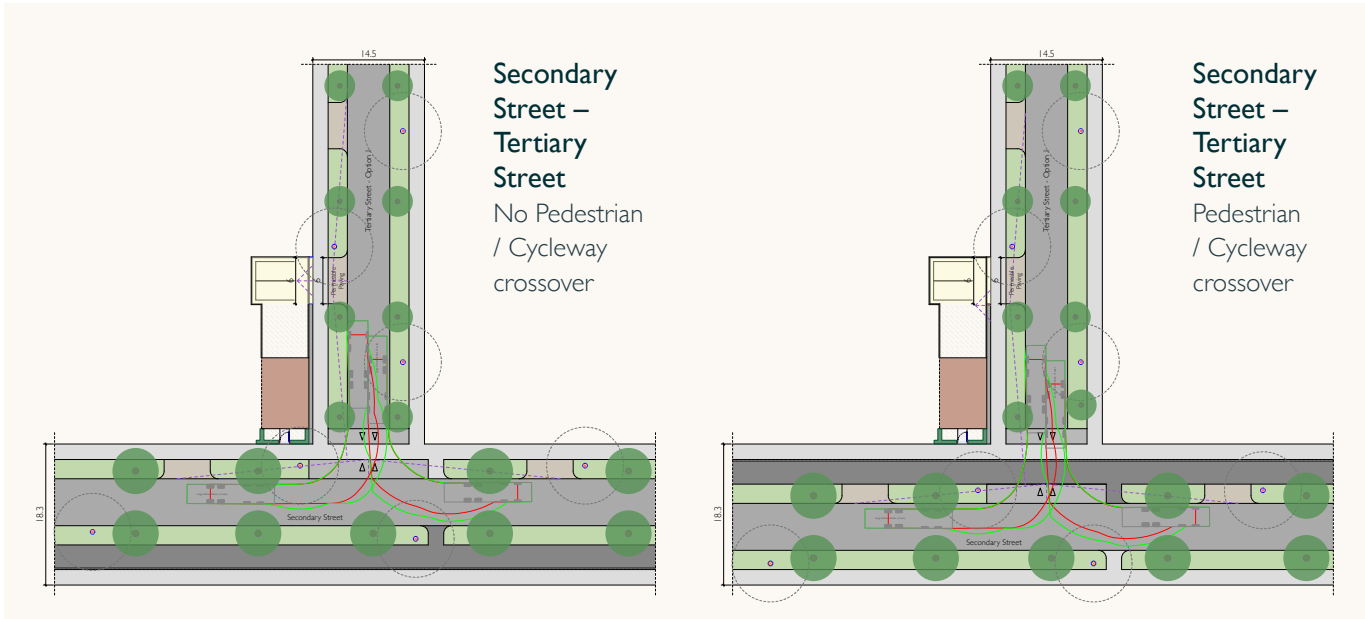
KEY
○ Key junction

7c. TYPICAL JUNCTIONS

Junctions have been designed to prioritise pedestrians and cyclists by the creation of crossovers. These occur on Welborne Way, primary

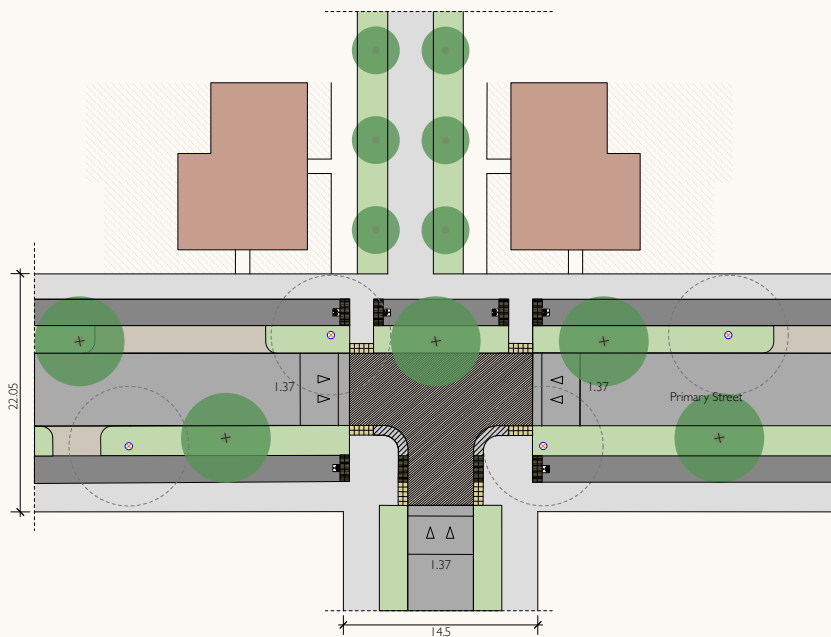
streets and secondary streets. The following diagrams show the typical street-type junctions.





7d. RAISED CROSSROADS & JUNCTIONS

The following diagrams show the typical raised crossroad and raised junction details.



Raised Junction Detail

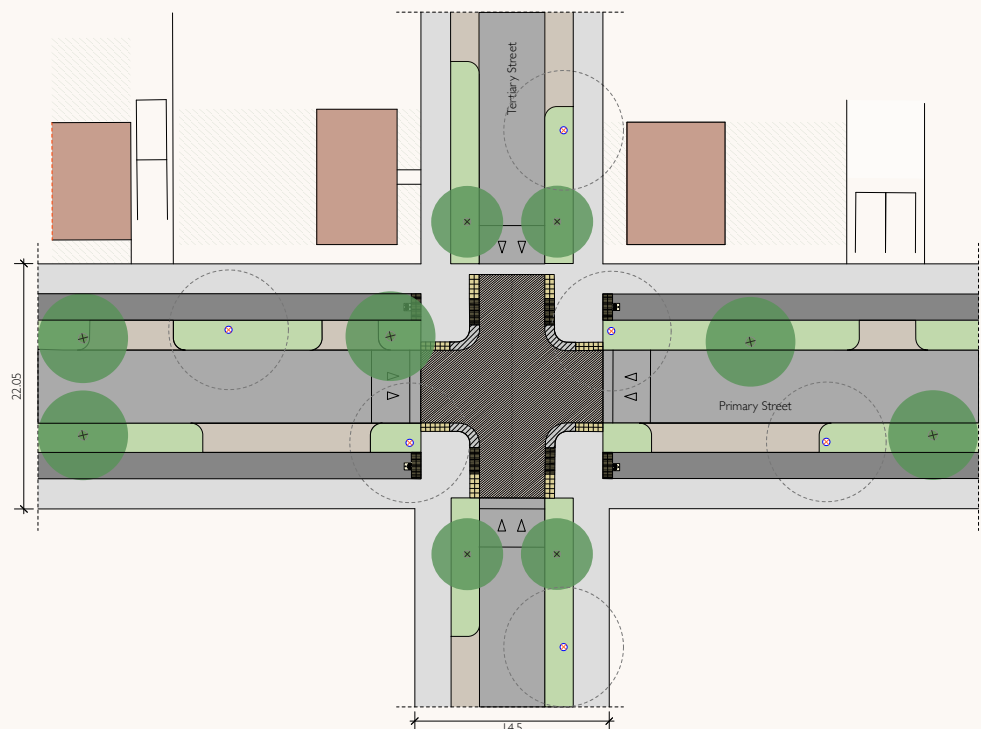
Pedestrian and cycle priority with level surface crossing and change of surface material

Resin bonded gravel to be used on primary and secondary streets for change of surface material. Setts maybe used on tertiary streets and edge lanes

Raised Crossroad Detail

Pedestrian and cycle priority with level surface crossing and change of surface material

Resin bonded gravel to be used on primary and secondary streets for change of surface material. Setts maybe used on tertiary streets and edge lanes

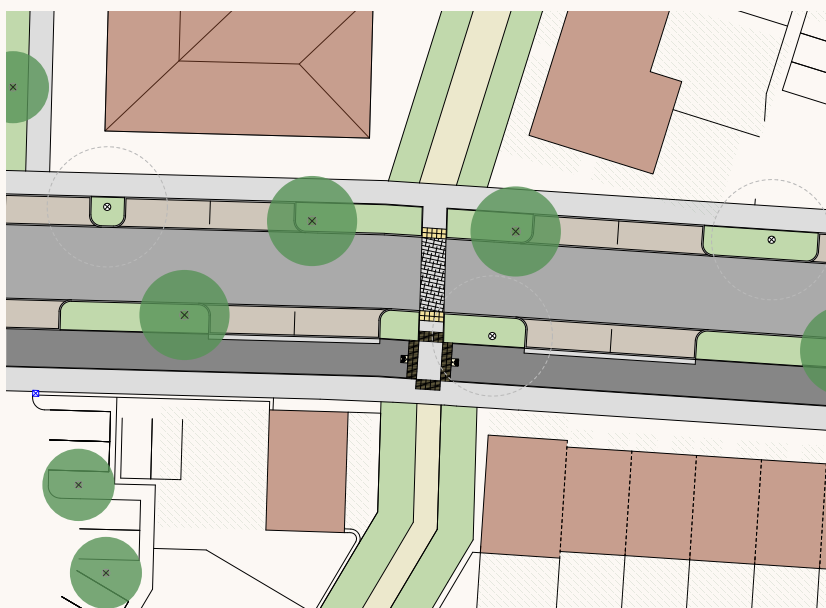


7e. TYPICAL CROSSING DETAILS

The following diagrams show typical instances where an important pedestrian route such as a green link crosses a street.

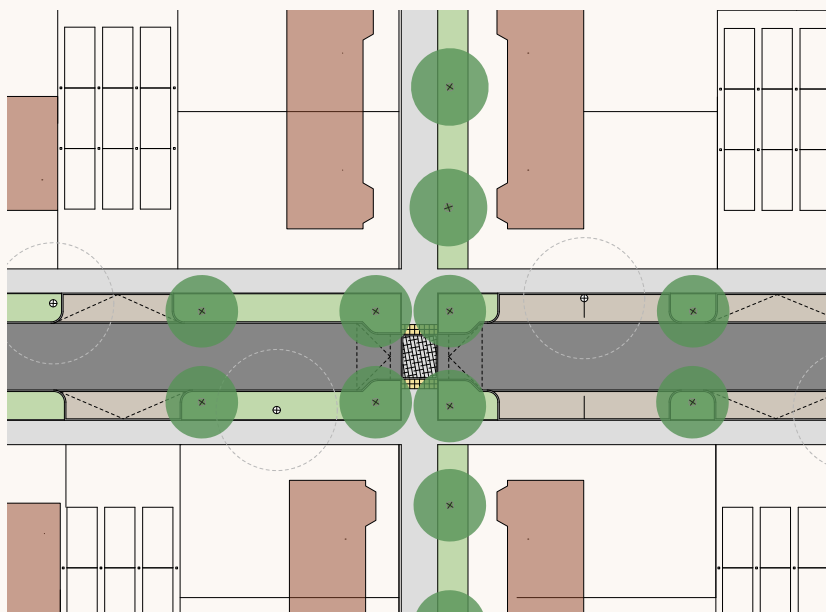
Primary/ Secondary Street Crossing

These will be identified with a change of surface (Resin bonded gravel)



Tertiary Street/ Edge Lane Crossing

These may include a raised surface with a narrowing and/ or a change of surface (Resin bonded gravel or Setts)

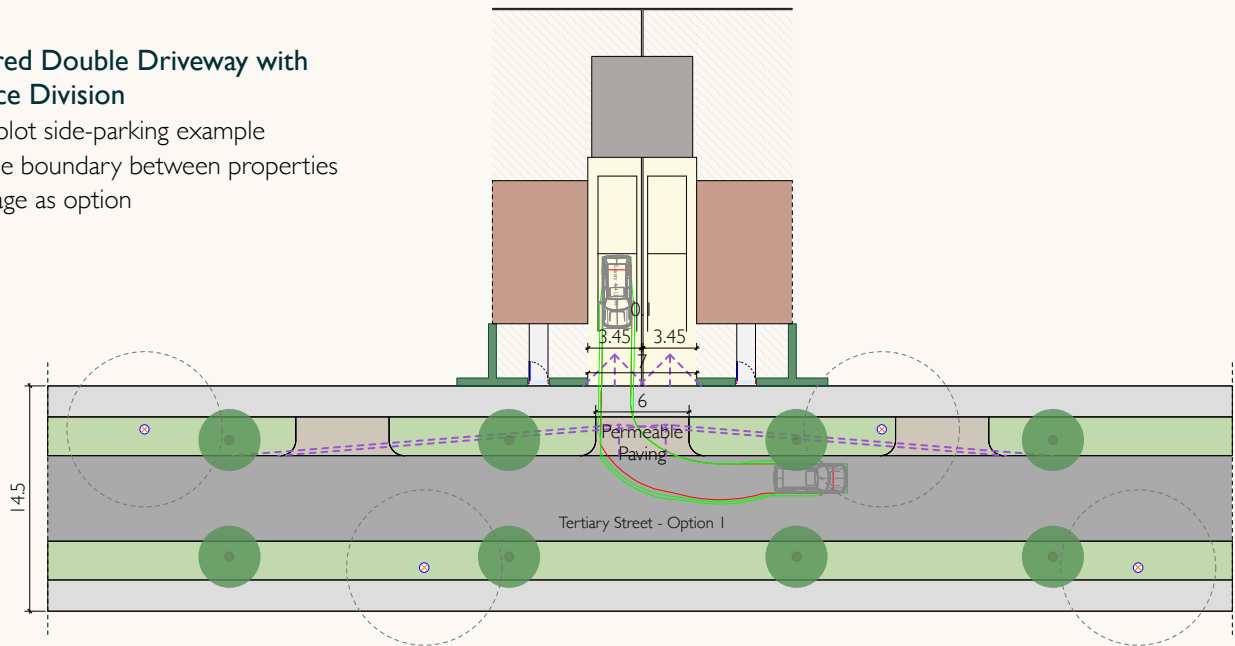


7f. TYPICAL DRIVEWAY DETAILS

The following diagrams show the typical driveway crossover details. Visibility splays are shown in accordance with the coding detailed in Section 5g.

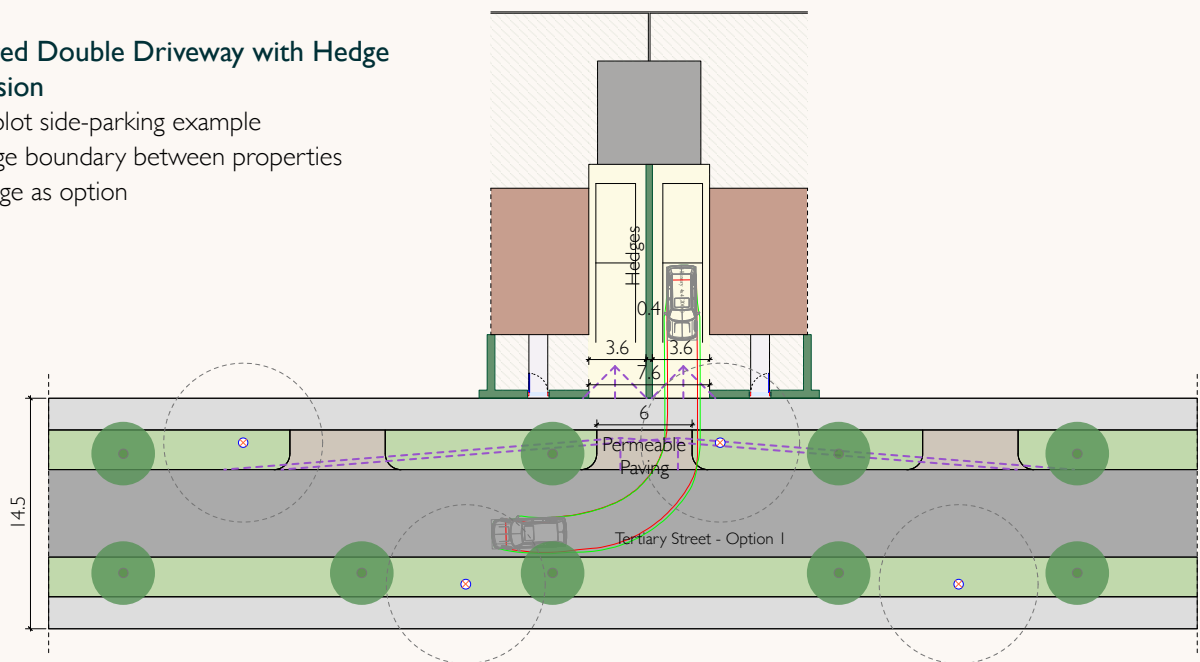
Shared Double Driveway with Fence Division

On-plot side-parking example
Fence boundary between properties
Garage as option



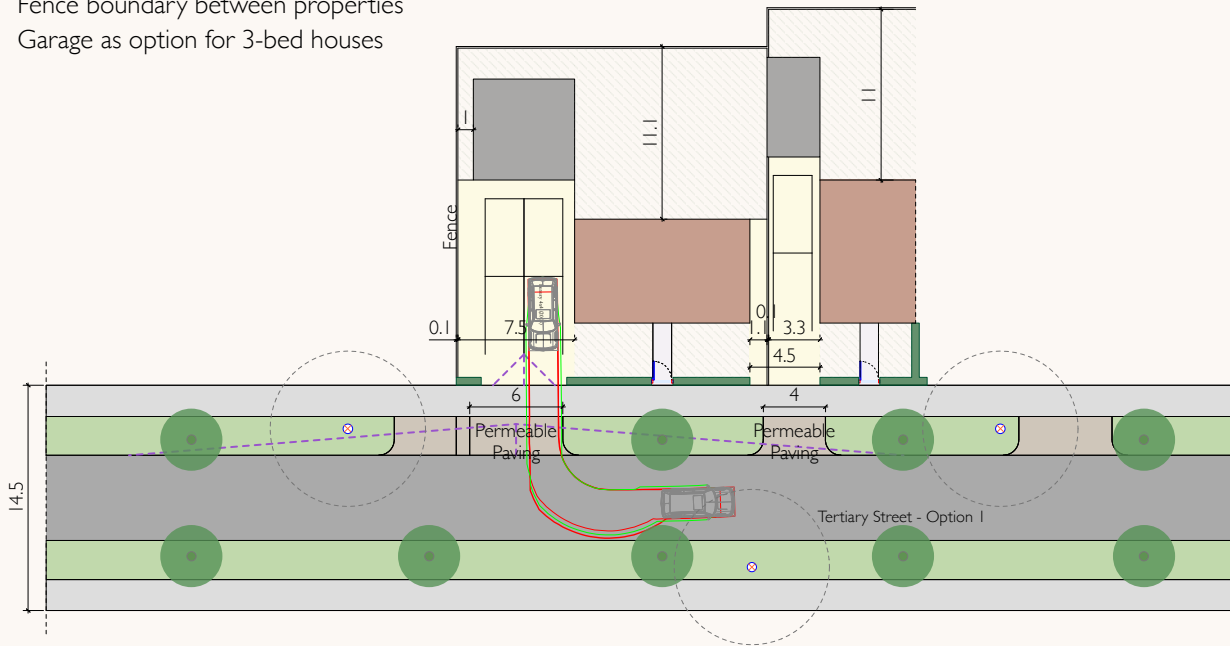
Shared Double Driveway with Hedge Division

On-plot side-parking example
Hedge boundary between properties
Garage as option



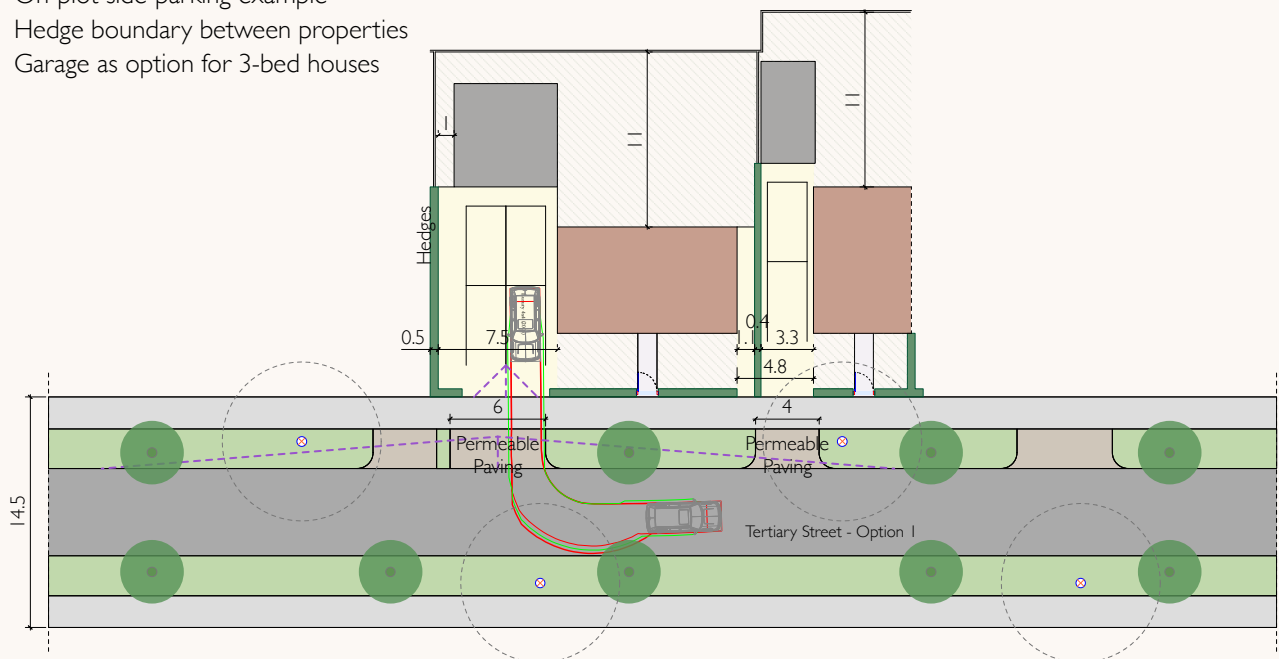
Detached Plot with Fence Boundaries

On-plot side-parking example
Fence boundary between properties
Garage as option for 3-bed houses



Detached Plot with Hedge Boundaries

On-plot side-parking example
Hedge boundary between properties
Garage as option for 3-bed houses



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Hampshire businesses and Inward Investment opportunities, in accordance with the approved Economic Strategy.

Contextual information

7. The County Council has been a member of AER since 2007, and has previously hosted a Brussels Office, formally the Southern England Local Partners (SELP). Following a Cabinet decision in January 2022, it was agreed to close the Brussels Office alongside agreement to consider the County Council’s on-going membership of the AER network.
8. The AER currently comprises 140 defined regions across 25 nations. Since 2019, Hampshire is currently the only UK region as a full Member of this body.
9. Historically the County Council was involved in shaping a range of projects and championing trade and investment. The most recent project, “Everywhere Internationals Small and Medium-sized Enterprises (SME’s) European Project”, led by Hampshire County Council, completed in September 2022. The final output from this programme is a report into the impacts of Covid-19 on SME’s, which was an extension to the original project.
10. Projects delivered through the membership in Hampshire include:
 - a. Everywhere International SMEs (EIS) 2017-22. Hampshire leads this project funded by Interreg Europe involving six other European regions with a total project budget of c. €1.5m (c.£1.3million).
 - b. Regional Adult Learning Multipliers & the Europe 2020 Strategy (REALM), 2011-12. Hampshire led this project funded under the Erasmus programme with a budget of c. €170k (c.£150,000)
11. The objectives of the County Council maintaining membership included making connections with European regions, which Hampshire businesses may find beneficial to expand their businesses into, and these links could provide support for the business to do so, as well as “selling” the benefits of locating and operating in Hampshire for businesses looking at expanding into the UK. It has not been possible to evidence any future direct benefits to the County Council nor Hampshire businesses, hence the closure of the Brussels Office and the recommendation to withdraw from AER.

Costs of Maintaining Membership

12. The resources and on-going costs of membership are outlined below

Expense	Cost per annum
Annual Membership	€11,000 (c£10,000)
General Assembly Meetings Twice per year 2 x Councillors 1 x Officer	c.£5,000 inclusive of expenses
Networking and Partnership Meetings 4 meetings per year	c.£8,000 inclusive of expenses

Minimum 1 Officer attending	
Officer Support 0.75 FTE* @ Grade G inclusive of on-costs	£43,109pa
Management Support 0.15 FTE @ Grade I inclusive of on-costs	£12,401pa**
Senior Officer Support 0.10 FTE across Director & Assistant Director inclusive of on costs	£16,500pa**
Project Costs No current projects being undertaken	Dependent on project and could be reclaimed through project grants and levels of match funding required.

*If more resource is required to deliver project(s), these costs could be reclaimed as part of the project grants.

**These are not cashable savings, but an example of the costs associated with supporting project delivery

13. **Officer Support** - The engagement in supporting the meeting schedule and associated project work is picked up by the Economic Development team, currently led by the Economy Strategy Manager. Resource levels will depend on the engagement in particular projects, but on a recurring basis this is estimated to be c0.75 FTE (Grade G), plus additional senior leadership oversight at Director and Assistant Director level of c0.10 FTE
14. **Future** membership of the AER requires a presence in the EU meetings with networking at key partner events. Without a presence in Brussels following the decisions previously taken, resource needs to be allocated to monitor funding and partnership calls for their relevance to Hampshire, as well as attending Assembly and Partnership meetings. None of the associated costs of attendance are currently budgeted for within the Economic Development budgets. Any projects undertaken would require alignment of the County Council's work with the objectives of the partnership project, which risks diluting the outcomes for Hampshire.
15. Post Brexit, the UK is not part of the formal EU structure, and it is unclear if the County Council would have any impact or influence on policy or funding proposals, albeit that the AER is not defined as a European Union focused platform.
16. Within the new Economic Strategy, the focus is to support businesses in international trading via exporting, working also alongside the Solent Freeport, enterprise and innovation zones as well as building local partnerships with businesses and organisations, supported through central Government funding and any potential form of devolution. It can therefore be viewed that continued membership may simply distract from the core work being undertaken locally within Hampshire.

Finance

17. The closure of the Brussels office in 2022 released c£0.1million of savings for SP23. As per the table in paragraph 11, a further £66,000 of savings could be achieved through not continuing with membership.

Consultation and Equalities

18. An Equality Impact Assessment has not identified any negative impacts following the resignation from the Assembly of European Regions on any protected group or geographic area within Hampshire.

Climate Change Impact Assessments

19. Hampshire County Council utilises two decision-making tools to assess the carbon emissions and resilience of its projects and decisions. These tools provide a clear, robust, and transparent way of assessing how projects, policies and initiatives contribute towards the County Council's climate change targets of being carbon neutral and resilient to the impacts of a 2°C temperature rise by 2050. This process ensures that climate change considerations are built into everything the Authority does.

Climate Change Adaptation

20. This proposal is not vulnerable to climate change.

Carbon Mitigation

21. Carbon emissions are only likely with continued membership of the AER through continued travel into Europe.

Other Key Issues

22. Clarification is currently being sort from the AER secretariat to confirm leaving processes in the context of the membership year running from January to December.
23. The membership agreement states that a letter must be sent to the AER prior to the 1st of January to cease membership, therefore a letter will have to be sent from the County Council to the AER secretariat prior to 31 December 2023 so this can be ratified by the Executive Board, and the County Council will formally be removed from the Assembly as of 1 January 2024.
24. The invoice for this current year's membership is yet to be received.

Conclusions

25. Resignation from the Assembly would ensure Economic Development resources are dedicated to supporting businesses within Hampshire and expanding the Inward Investment offer supporting Growth. Export support for businesses will be continued and investigation and support to access overseas markets will be available to businesses through partnerships.

REQUIRED CORPORATE AND LEGAL INFORMATION:

Links to the Strategic Plan

Hampshire maintains strong and sustainable economic growth and prosperity:	yes
People in Hampshire live safe, healthy and independent lives:	no
People in Hampshire enjoy a rich and diverse environment:	no
People in Hampshire enjoy being part of strong, inclusive communities:	no

Other Significant Links

Links to previous Member decisions:	
<u>Title</u> Hampshire Brussels Office Hampshire Brussels Office-2022-01-27-EMETE Decision Day (hants.gov.uk)	<u>Date</u> 27 th January 2022
Direct links to specific legislation or Government Directives	
<u>Title</u>	<u>Date</u>

Section 100 D - Local Government Act 1972 - background documents	
<p>The following documents discuss facts or matters on which this report, or an important part of it, is based and have been relied upon to a material extent in the preparation of this report. (NB: the list excludes published works and any documents which disclose exempt or confidential information as defined in the Act.)</p>	
<u>Document</u>	<u>Location</u>
None	

EQUALITIES IMPACT ASSESSMENT:

1. Equality Duty

The County Council has a duty under Section 149 of the Equality Act 2010 ('the Act') to have due regard in the exercise of its functions to the need to:

- Eliminate discrimination, harassment and victimisation and any other conduct prohibited by or under the Act with regard to the protected characteristics as set out in section 4 of the Act (age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation);
- Advance equality of opportunity between persons who share a relevant protected characteristic within section 149(7) of the Act (age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation) and those who do not share it;
- Foster good relations between persons who share a relevant protected characteristic within section 149(7) of the Act (see above) and persons who do not share it.

Due regard in this context involves having due regard in particular to:

- The need to remove or minimise disadvantages suffered by persons sharing a relevant characteristic connected to that characteristic;
- Take steps to meet the needs of persons sharing a relevant protected characteristic different from the needs of persons who do not share it;
- Encourage persons sharing a relevant protected characteristic to participate in public life or in any other activity which participation by such persons is disproportionately low.

2. Equalities Impact Assessment:

An Equality Impact Assessment has not identified any negative impacts following the resignation from the Assembly of European Regions on any protected group or geographic area within Hampshire.

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HAMPSHIRE COUNTY COUNCIL

Decision Report

Decision Maker:	Leader and Executive Member for Hampshire 2050 and Corporate Services
Date:	9 March 2023
Title:	The Leader's Community Grant Fund and Investing in Hampshire Fund
Report From:	Director of Hampshire 2050

Contact name: Emma Noyce

Tel: 0370 779 8373

Email: emma.noyce@hants.gov.uk

Purpose of this Report

1. The report considers four applications for a grant from the Leader's Community Grant Fund 2022/23 and a request to carry forward funds awarded from the Investing in Hampshire Fund to 2023/24.

Recommendations

2. That in respect of the grant application from Odiham Parish Council, and for the reason set out in this report, the Leader and Executive Member for Hampshire 2050 and Corporate Services amends the Leader's Community Grants scheme conditions by waiving the restriction preventing an award being made to a Parish Council with regards to the application in this report only.
3. That the Leader and Executive Member for Hampshire 2050 and Corporate Services approves four grants totalling £20,500 from the Leader's Community Grant Fund 2022/23 to cultural and community organisations in Hampshire as outlined in Appendix 2.
4. That the Leader and Executive Member for Hampshire 2050 and Corporate Services approves the carry forward of part of the grant award from the Investing in Hampshire Fund to The Bursledon Brickworks Museum to 2023/24.

Executive Summary

5. Four organisations have applied for a grant through the Leader's Community Grant Fund 2022/23 and the funds requested total £25,425. The report considers the applications and recommends the awards totalling £20,500. The recommended award can be met from within existing budget provision.
6. Bursledon Brickworks Museum received an award of £100,000 from the Investing in Hampshire Fund in 2021. The works have been delayed and only a proportion of the funds will have been claimed within the year of the award. Therefore, a request has been made to carry forward the remaining funds to 2023/24 when the works will be completed.

Contextual Information

7. The purpose of this grant stream is to fund projects which provide community benefit and help local communities thrive and/or to help local organisations become financially self-supporting and not reliant on public sector funding.

Full details, including what the County Council can and cannot fund, are set out in Appendix 1.

8. Four applications have been submitted for consideration. Details are set out in Appendix 2.
9. It is noted that applications from Parish Councils are not normally funded under this Scheme, as set out in the grant criteria. However, it is considered appropriate to apply an exception to the application from Odiham Parish Council due to the high costs associated with improvements to the building following its transfer of the freehold from Hampshire County Council on 1 March 2022 to become an independent community library.
10. On 8 July 2021, the Executive Member for Policy and Resources approved an award of £100,000 from the Investing in Hampshire Fund towards Phase 1 works at the Bursledon Brickworks Museum. The award was on condition that the remaining funding being in place. This was confirmed last year as coming from Historic England. Scheduling the works has proved challenging since the pandemic but works are now progressing. Arrangements have been made for staged payments aligned to the various milestones of the project, however this will mean that some of the grant award will not be claimed until after a year since the award was accepted. As it is a condition of the grant to be claimed within a year, approval is sought to carry forward the outstanding amount to 2023/24.

Finance

11. The recommended awards can be met from within existing budget provision.

Consultation and Equalities

12. A high-level Equalities Impact Assessment has been undertaken. The grants are intended to have a positive impact and advance equality.
13. The corporate terms and condition of grant require that any organisation in receipt of funding shall ensure that at all times it complies with the Equality Act 2010 if applicable and shall ensure that it does not discriminate against any person or persons on the basis of protected characteristics.

Climate Change Impact Assessment

14. Hampshire County Council utilises two decision-making tools to assess the carbon emissions and resilience of its projects and decisions. These tools provide a clear, robust, and transparent way of assessing how projects, policies and initiatives contribute towards the County Council's climate change targets of being carbon neutral and resilient to the impacts of a 2°C temperature rise by 2050. This process ensures that climate change considerations are built into everything the Authority does.
15. The carbon mitigation tool decision tree indicates it is not suitable for the assessment of a programme. The decisions in this report are financial decisions in relation to a programme of one-off grant opportunities. Therefore, the tool is not suitable for this Climate Change Impact Assessment and has not been used.

Other Key Issues

16. Legal Implications: Section 1 (1) of the Localism Act gives the County Council the power to do anything that individuals may generally do. This includes the power to make grants.

REQUIRED CORPORATE AND LEGAL INFORMATION:

Links to the Strategic Plan

Hampshire maintains strong and sustainable economic growth and prosperity:	no
People in Hampshire live safe, healthy and independent lives:	yes
People in Hampshire enjoy a rich and diverse environment:	yes
People in Hampshire enjoy being part of strong, inclusive communities:	yes

Other Significant Links

Links to previous Member decisions:	
<u>Title</u>	<u>Date</u>
Leader's Community Grants – Revised Criteria and Management	8 July 2021

Section 100 D - Local Government Act 1972 - background documents

The following documents discuss facts or matters on which this report, or an important part of it, is based and have been relied upon to a material extent in the preparation of this report. (NB: the list excludes published works and any documents which disclose exempt or confidential information as defined in the Act.)

<u>Document</u>	<u>Location</u>
None	

EQUALITIES IMPACT ASSESSMENT:

1. Equality Duty

The County Council has a duty under Section 149 of the Equality Act 2010 ('the Act') to have due regard in the exercise of its functions to the need to:

- Eliminate discrimination, harassment and victimisation and any other conduct prohibited by or under the Act with regard to the protected characteristics as set out in section 4 of the Act (age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation);
- Advance equality of opportunity between persons who share a relevant protected characteristic within section 149(7) of the Act (age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation) and those who do not share it;
- Foster good relations between persons who share a relevant protected characteristic within section 149(7) of the Act (see above) and persons who do not share it.

Due regard in this context involves having due regard in particular to:

- The need to remove or minimise disadvantages suffered by persons sharing a relevant protected characteristic that are connected to that characteristic;
- Take steps to meet the needs of persons sharing a relevant protected characteristic that are different from the needs of persons who do not share it;
- Encourage persons sharing a relevant protected characteristic to participate in public life or in any other activity in which participation by such persons is disproportionately low.

2. Equalities Impact Assessment:

- 2.1. A high-level Equalities Impact Assessment has been undertaken. The grants are intended to have a positive impact and advance equality.

Leader's Community Grants – Criteria

What is funded?

The purpose of the scheme is to fund one-off, time limited projects which:

- provide community benefit and help local communities thrive
- help local organisations become financially self-supporting and not reliant on public sector funding

Proposals will be particularly welcomed where they can demonstrate how they:

- respond to the [Climate Emergency](#), as declared by the Council in June 2019
- reduce demand for Council services

Proposals must support the priorities and outcomes of Hampshire County Council's [Serving Hampshire - Strategic Plan for 2021 - 2025](#). It forms the cornerstone of all strategies and plans across departments and service areas. It features four key aims:

- Hampshire maintains strong and sustainable economic growth and prosperity
- people in Hampshire live safe, healthy and independent lives
- people in Hampshire enjoy a rich and diverse environment
- people in Hampshire enjoy being part of strong, inclusive communities

Applications can be made for over £1,000 to a maximum of £25,000 and may be for activity costs and/or capital costs. Applications in excess of £25k will occasionally be considered in exceptional circumstances.

Secured match funding or clear evidence of seeking match funding is highly desirable. Organisations should have also applied to local District or Parish Councils where local funding opportunities exist.

Supporting documentation will be required for all applications.

Who can apply?

Organisations wishing to apply must be properly constituted with clear and effective governance and management structures. They must be delivering activities or services that benefit Hampshire residents. Organisations will not normally be eligible for grants where they hold unallocated reserves in excess of one year's running costs. Where it is judged that unallocated reserves are unreasonably in excess of what is required or not allocated for legitimate purposes, those organisations may receive a reduced grant. All organisations are expected to have an agreed reserves policy that addresses their financial risk.

The following organisations are not normally funded under this scheme:

- individuals
- services provided solely within the unitary authority areas of Portsmouth City Council and/or Southampton City Council

- other local authorities, including District, Parish and Town Councils
- commercial/profit making organisations
- schools, including pre-schools and colleges
- large, national organisations or 'household name charities', including local branches
- political parties or groups affiliated with a political party or from lobbying/campaigning organisations.
- Applications will only be considered from faith groups who can demonstrate their facilities are open and used by the local community other than for church related activities and then only for works in relation to the Disability Discrimination Act
- Requests may be considered from County wide groups (e.g. Guides, Scouts, Hampshire County Youth Orchestra) for specific trips e.g. for an international jamboree or cultural tour. Individual requests will not usually be considered.
- Self-help by organisations is encouraged, and therefore grants are normally only a percentage of the sum required to complete a project.

When to apply

Applications are welcomed at any time and will be dealt with on a 'first come, first served' basis. Recommendations for awards will be taken to the next available Executive Members decision day. This may be up to three months after the application is received, in line with meeting schedules.

Projects should be well developed, with match-funding secured, and any supported projects will normally be expected to commence within six-months of the decision date. All applicants for building works will be expected to evidence freehold or to have obtained permission from the landlord. If required, applicants will also be expected to have obtained (or engaged in the process of) planning permission and/or listed buildings consent (if appropriate) to carry out the works before applying.

What is not funded?

The following projects are not funded under this scheme:

- the purchase of land
- capital works that have already been carried out - we cannot retrospectively fund projects; grants are awarded for works to be carried out in the future
- general running costs of organisations - these grants are not intended to replace shortfalls in service delivery budgets or replace funding for other support you may be able to obtain, or that you have obtained from us in the past
- ongoing staff costs - we may fund a fixed-term member of staff or consultant to carry out a time-limited piece of work
- repeat applications to cover previously funded activities/annual applications for the same activities
- profit-making or fund-raising activities, or onward distribution of funds

- projects which meet the aims of other Hampshire County Council grant schemes - where relevant, applications may be transferred to more appropriate schemes
- applications for £1,000 or less - where relevant, these should be directed to the Members' Devolved Grants Scheme.

Organisations will not be awarded more than one grant per financial year and previously funded projects must be completed before a new application is made. Previous levels of Council funding will be taken into account when assessing applications.

Grant Funding to Culture and Community Organisations - 2022/23

Organisation (District / Division)	Proposal	Amount Requested	Amount Recommended
Odiham Parish Council		<i>(Hart/Odiham & Hook)</i>	
<i>Application summary</i>	<p>The Parish Council took on the freehold of the former Odiham Library building from the County Council in March 2022 and seeks funding towards capital works to transform it into a community hub. This will include office premises for the Parish Council together with other facilities such as a Book Exchange and community hire for activities, support groups and training sessions. In addition, the local MP and Police will rent office space in the building for their use.</p> <p>Works include addressing a longstanding damp problem with the north facing wall. This application also includes small landscaping works to dig a trench, clear shrubs and remove leaves on the north-west wall to discourage water ingress on this boundary wall.</p> <p>In 2020 it was agreed that all libraries transferring from the County Council to Independent Community Libraries would be eligible for a grant of up to £10,000 to support transfer costs and £10,000 to support capital works associated with the transfer. Due to the age, condition and Grade 2 listed status of the building it is considered appropriate to allow a further £10,000 to support these works to be applied for.</p> <p>Note: £10,000 Recreation and Heritage Community Fund grant awarded 12/01/21 & £10,000 Recreation and Heritage Community Fund grant awarded 21/06/21 towards the transition to an independent community library.</p>	£9,925	£10,000
<i>Reach</i>	Will benefit approx. 2000 Hampshire residents.		
<i>Funding (inc match)</i>	The total project costs are £10,625. Own funds committed: £700.		
<i>Local Member Comments</i>	Councillor Glen supports the applicaiton.		

Organisation (District / Division)	Proposal	Amount Requested	Amount Recommended
Recommendation/ conditions	<p>It is recommended the organisation is awarded £10,000.</p> <p>It is noted that Parish Councils are not normally eligible to apply to this grant scheme but it is recommended that this application is agreed by exception due to the high costs involved with the transfer of the building. This will be the last award to this project through this grant scheme.</p>		

Organisation (District / Division)	Proposal	Amount Requested	Amount Recommended
Whitehill Village Hall			
<i>(East Hampshire/Whitehill, Bordon and Lindford)</i>			
Application summary	<p>The application seeks funding towards the replacement of the roof of the main hall.</p> <p>The hall supports a range of community social and recreational activities for all ages including fitness and healthy living activities, children's activities, soup kitchen, U3A, breastfeeding support and friendship groups.</p> <p>The organisation is well-established and has delivered capital improvement projects previously. It has committed £15,000 of its own funds towards the project. It was unsuccessful in application/s to EHDC District Councillors Discretionary grants due to no funds remaining. It has previously accessed funding from EHDC including Section 106 funding, to support other recent capital improvements.</p>	£10,000	£5,000
Reach	Will benefit approx. 1200 Hampshire residents.		
Funding (inc match)	<p>The total project costs are £27,900. Own funds committed: £15,000; Councillor Grant - £367</p> <p>Note: £9,910 - Community Buildings Capital Fund 16/7/2015; £3,016 - Councillor Grant 23/10/2015 £3,000 - Culture & Community Activity Grants 21/01/2016;</p>		

Organisation (District / Division)	Proposal	Amount Requested	Amount Recommended
	£9,910 - Community Buildings Capital Fund 21/03/2017; £1,700 - Councillor Grant 17/02/2017; £10,000 - Community Buildings Capital Fund 25/02/2019; £1,300 – Councillor Grant 19/07/2019; £745 – Councillor Grant 16/02/2022; Request for £3,000 – Councillor Grant – Decision Pending.		
Local Member Comments	Councillor Tree supports the application.		
Recommendation/ conditions	It is recommended the organisation is awarded a partial award of £5,000 and the organisation advised to seek funds from other sources in the future so not to be reliant on Hampshire County Council grant funding. This will be the last award to this organisation through this grant scheme.		

Organisation (District / Division)	Proposal	Amount Requested	Amount Recommended
Portsmouth Harbour Marine			
		<i>(Gosport/Multiple Divisions)</i>	
Application summary	<p>The application seeks funding towards Gosport Marine Festival 2023 which will take place on 20 May. This is a free community event, showcasing the wealth of on-water opportunities available on the Gosport peninsula and allowing people of all ages and abilities to try a variety of these activities. The festival also focuses on the young people of Gosport, highlighting the learning and career opportunities within the marine industry on the peninsula. The biennial festival has been running since being launched in 2015.</p> <p>On the water activities will include: dinghy sailing, rowing, kayaking, sail training vessels, Wetwheels Solent disabled boat trips, visits to historical vessels in the Marina, under 6 model yacht race, Portsmouth model boat display team. On shore activities will include: Programme of entertainment throughout the day, under 6 little shipmates' activities, Maritime careers exhibition, sea cadet display area.</p>	£3,000	£3,000

Organisation (District / Division)	Proposal	Amount Requested	Amount Recommended
	The requested funding will go towards marketing costs, other associated costs for putting the event on and also towards the Marine Futures program, which gives young people who attend the festival the opportunity to expand their sailing abilities with a week sail training to ensure a lasting legacy of the event for those who participate.		
<i>Reach</i>	Will benefit approx. 2500 Hampshire residents.		
<i>Funding (inc match)</i>	The total project costs are £13,050. National Lottery - £2250 (outcome awaited); Boost Charitable Trust - £750 (outcome awaited); Councillor Grants - £1,500 (see note below). Note: The organisation has secured a £500 Councillor Grant and the application indicates they expect to receive a further 2x £500 awards bringing the total CC grants to £1,500. The amount requested in this application would still bring HCC's contribution to less than 50% of project costs. County Councillors supported the 2021 event (4x awards totalling £2,500) and these grants are a strong proxy indicator of local value and support.		
<i>Local Member Comments</i>	Local Councillors support this application.		
<i>Recommendation/ conditions</i>	It is recommended the organisation is awarded £3,000.		

Organisation (District / Division)	Proposal	Amount Requested	Amount Recommended
The Hampshire Duke of Edinburgh's Forum			(Countywide)

Organisation (District / Division)	Proposal	Amount Requested	Amount Recommended
Application summary	The Hampshire DoE Forum, a local voluntary group established to support and promote the DofE scheme in Hampshire, hosts an annual awards event for recipients of the DoE Gold Award. The event recognises and celebrates young people's achievement and serves to promote the benefits of participating in the DoE scheme. For a number of years, the event has taken place in the Great Hall, hosted by HCC. To ensure transparency, the Forum have been asked to submit a grant application to cover the costs of hiring the Great Hall at the HCC 'partner rates'. The event covers all young people in Hampshire who are undertaking their DoE Award with a local school or other licenced DoE authority and is non-subscription.	£2,500	£2,500
Reach	Will benefit approx. 200 Hampshire residents.		
Funding (inc match)	The total project costs are £2,500. Note: £2,000 Leader's Community Grant Fund – 9/3/22		
Local Member Comments	Multiple Divisions.		
Recommendation/ conditions	It is recommended that funding of £2,500 is awarded to cover the hire of the Great Hall for the 2023 annual awards ceremony. The organisation is advised to find an alternative venue for future events. (Note: The Forum is not formally associated with the DoE national charity or funded by HCC.)		
TOTALS		£25,425	£20,500

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HAMPSHIRE COUNTY COUNCIL

Decision Report

Decision Maker:	Leader and Executive Member for Hampshire 2050 and Corporate Services
Date:	9 March 2023
Title:	Parish and Town Council Investment Fund - Approval of Larger Grants
Report From:	Director of Hampshire 2050

Contact name: Lisa Wood, Rural Programme Support Officer

Tel: 0370 779 5211

Email: Lisa.wood@hants.gov.uk

Purpose of this Report

1. The purpose of this report is to request approval from the Executive Member for Hampshire 2050 and Corporate Services for an application to the Parish and Town Council Investment Fund (PTCIF) from Church Crookham Parish Council.

Recommendation

2. That the Leader and Executive Member for Hampshire 2050 and Corporate Services approves the proposed grant application to the PTCIF as detailed in Appendix A.

Executive Summary

3. Details of the grant application, for which this paper seeks approval, are given in Appendix A. The total amount requested is £19,553.

Contextual Information

4. The Rural Programme supports the County Council's support of rural Hampshire and assisting Parish and Town Councils to respond to local priorities, including management of the [PTCIF grant scheme](#).

5. The priority of the PTCIF is to support partnerships between Parish and Town Councils or local community groups and the County Council. It provides investments to schemes which respond to local needs and aims to reduce pressure on County Council Services.
6. Hampshire County Council declared a Climate Emergency in June 2019. In November 2021 it was agreed that the PTCIF scheme guidance would be extended to include Energy Efficiency applications from Parish and Town Councils or Community Groups for improvements to the energy efficiency of community buildings, where it can be demonstrated that the improvements contribute to [HCC's climate change targets](#).
7. Each applicant to the PTCIF is required to submit a full business plan, to include rationale, costings, quotes, and measurements. For Energy Efficiency applications, this business plan needs to provide adequate information to the Climate Programme Team for them to evaluate the financial and carbon footprint benefits of the project. If these evaluations are favourable, and demonstrate a contribution to Hampshire County Council's Climate Emergency targets, then the project is supported by that team, and will be recommended for funding here.

Finance

8. This grant application can be met from the existing PTCIF budget.

Performance

9. These grants have been reviewed by the Climate Programme Team and the Rural Programme Team respectively, against the grant scheme criteria detailed via the link in Section 4, and approval is only sought following their recommendation to support.
10. The detailed assessment is provided in Appendix A.

REQUIRED CORPORATE AND LEGAL INFORMATION:

Links to the Strategic Plan

Hampshire maintains strong and sustainable economic growth and prosperity:	yes
People in Hampshire live safe, healthy and independent lives:	yes
People in Hampshire enjoy a rich and diverse environment:	yes
People in Hampshire enjoy being part of strong, inclusive communities:	yes

Other Significant Links

Links to previous Member decisions:	
<u>Title</u>	<u>Date</u>
EMPR Report - Update on the Parish and Town Council Investment Fund and Associated Rural Issues	25.04.2019
Direct links to specific legislation or Government Directives	
<u>Title</u>	<u>Date</u>
HCC Climate Change Initiatives	13 July 2021

Section 100 D - Local Government Act 1972 - background documents

The following documents discuss facts or matters on which this report, or an important part of it, is based and have been relied upon to a material extent in the preparation of this report. (NB: the list excludes published works and any documents which disclose exempt or confidential information as defined in the Act.)

<u>Document</u>	<u>Location</u>
None	

EQUALITIES IMPACT ASSESSMENT:

1. Equality Duty

The County Council has a duty under Section 149 of the Equality Act 2010 ('the Act') to have due regard in the exercise of its functions to the need to:

- Eliminate discrimination, harassment and victimisation and any other conduct prohibited by or under the Act with regard to the protected characteristics as set out in section 4 of the Act (age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation);
- Advance equality of opportunity between persons who share a relevant protected characteristic within section 149(7) of the Act (age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation) and those who do not share it;
- Foster good relations between persons who share a relevant protected characteristic within section 149(7) of the Act (see above) and persons who do not share it.

Due regard in this context involves having due regard in particular to:

- The need to remove or minimise disadvantages suffered by persons sharing a relevant protected characteristic that are connected to that characteristic;
- Take steps to meet the needs of persons sharing a relevant protected characteristic that are different from the needs of persons who do not share it;
- Encourage persons sharing a relevant protected characteristic to participate in public life or in any other activity in which participation by such persons is disproportionately low.

2. Equalities Impact Assessment: No negative impacts are anticipated.

Appendix A – Church Crookham Zebon Centre

Parish and Town Council Investment Fund 2022/23

Funding assessment form

Organisation name:	Church Crookham Parish Council
Project title:	Zebon Centre – Solar
Project location:	Church Crookham
Application reference:	CG00026073
Total project cost:	£20,353
Amount requested:	£19,553
<i>Match funding – cash:</i>	
<i>Match funding - volunteer hours:</i>	£800
<i>Match funding - in-kind:</i>	
Match funding – total value	£800
AMOUNT RECOMMENDED:	£19,553

Eligibility checklist	Yes/No
Is the applicant a Parish or Town Council, or a Local Community Group, directly benefitting the local community, with clear management structure within its constitution or suitable governance/terms of reference?	Yes
Does the project/activity deliver against the scheme priorities (to build community resilience and ease pressure on HCC Services)?	Yes
Does the application have support from the local community and Parish/Town Council?	Yes

Project summary
<p>Church Crookham Parish Council intend to capitalise on the fabric improvements already completed at the Zebon Centre and wish to install a solar PV and battery storage system to allow the building to enjoy energy independence from the self-generation of their electricity supply.</p> <p>They have adopted a fabric first approach with the following improvements already completed.</p> <ul style="list-style-type: none"> • LED lighting installed throughout the centre – this will be reflected in a new EPC when we request one after this project is completed. • PIR sensors installed in main areas where lighting is likely to be left on by mistake - toilets in the main building and in the corridor, toilets and changing rooms of the pavilion end of the building. • After some accidental damage, we established that the top half (opening sections) of the windows in the 2 halls needed replacing. They were replaced with A-rated toughened glass • Old external timber cladding at the pavilion end of the building replaced with PVCu cladding in November 2021 • Smart meters installed to monitor Gas and Electricity supply

HCC – Officer feedback/comments (relevant Services)

Lisa Wood - Rural Programme Team:

Church Crookham Parish Council have already completed a number of energy-saving measures. This application seeks funding from the Parish and Town Council Investment Fund in line with HCC's energy efficiency grant funding. Their 'fabric first' approach has ensured that all elements of energy saving within the building have already been addressed.

Subject to review by Chris Dear I would support this application.

Chris Dear - Climate Emergency Dept:

Church Crookham Parish Council's Zebon Community Centre Energy project demonstrates their continuing commitment to the climate emergency, as declared by Hampshire County Council in 2019. Building on interventions already implemented to decarbonise the community centre, this application to install solar PV and a battery storage system is anticipated to further decarbonise the building. The project is set to achieve a substantial energy saving resulting in an annual financial saving of £1,758, in addition to an annual carbon saving of 1.43tCO₂e.

On that basis, the application is supported.

Cllr Tim Davies is aware of the application.

Officer comments and recommendation

Lisa Wood – Rural Affairs Programme Support Officer

I would recommend award of £19,553.

HAMPSHIRE COUNTY COUNCIL

Decision Report

Decision Maker:	Leader and Executive Member for Hampshire 2050 and Corporate Services
Date:	09 March 2023
Title:	Chief Officer Delegations Register
Report From:	Director of Universal Services, Hampshire 2050 and Director of People and Organisation

Contact name: Gary Westbrook, Director of Hampshire 2050

Tel: 0370 779 8940

Email: gary.westbrook@hants.gov.uk

Purpose of this Report

1. The purpose of this report is to redelegate existing Executive Member Authority in favour of the Director of Economy, Transport, and Environment (ETE), to the Director of Universal Services.

Recommendation

2. That the Leader and Executive Member for Hampshire 2050 and Corporate Services approves the list of time limited delegations to the Director of Economy, Transport and Environment in Appendix 1 and confirms that these are delegated to the Director of Universal Services.

Executive Summary

3. This report is required as a result of the Council's restructure on 1 January 2023 to ensure that existing delegations remain lawful and so that the new Directors of Hampshire 2050 and Universal Services can effectively and lawfully make decisions using the delegations.

Contextual information

4. On 1 January, CCBS, ETE and Law and Governance were disbanded, and their functions primarily reallocated as appropriate to Hampshire 2050, Universal Services and People and Organisation.
5. In order to ensure the effective running of the Council, existing delegations to the Director of ETE listed in the appendix should be reconfirmed to the Director of Universal Services.

6. Links to the relevant reports in the Appendices set out the rational for and the detail of each delegation.

Climate Change Impact Assessment

7. Hampshire County Council utilises two decision-making tools to assess the carbon emissions and resilience impacts of its projects and decisions. These tools provide a clear, robust, and transparent way of assessing how projects, policies and initiatives contribute towards the County Council's climate change targets of being carbon neutral and resilient to the impacts of a 2°C temperature rise by 2050. This process ensures that climate change considerations are built into everything the Authority does.
8. The carbon mitigation tool and/or climate change adaptation tool are not applicable because the decision is administrative in nature.

REQUIRED CORPORATE AND LEGAL INFORMATION:

Links to the Strategic Plan

Hampshire maintains strong and sustainable economic growth and prosperity:	yes
People in Hampshire live safe, healthy and independent lives:	yes
People in Hampshire enjoy a rich and diverse environment:	yes
People in Hampshire enjoy being part of strong, inclusive communities:	yes

Section 100 D - Local Government Act 1972 - background documents	
<p>The following documents discuss facts or matters on which this report, or an important part of it, is based and have been relied upon to a material extent in the preparation of this report. (NB: the list excludes published works and any documents which disclose exempt or confidential information as defined in the Act.)</p>	
<u>Document</u>	<u>Location</u>
None	

EQUALITIES IMPACT ASSESSMENT:

1. Equality Duty

The County Council has a duty under Section 149 of the Equality Act 2010 ('the Act') to have due regard in the exercise of its functions to the need to:

- Eliminate discrimination, harassment and victimisation and any other conduct prohibited by or under the Act with regard to the protected characteristics as set out in section 4 of the Act (age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation);
- Advance equality of opportunity between persons who share a relevant protected characteristic within section 149(7) of the Act (age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation) and those who do not share it;
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Due regard in this context involves having due regard in particular to:

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- Take steps to meet the needs of persons sharing a relevant protected characteristic that are different from the needs of persons who do not share it;
- Encourage persons sharing a relevant protected characteristic to participate in public life or in any other activity in which participation by such persons is disproportionately low.

2. Equalities Impact Assessment:

2.1. There are no equality impacts arising as a result of this Report

Directorate Register of Delegations (for Hampshire 2050 Directorate)

PART B : TIME LIMITED DELEGATIONS

Date of Delegation	Name of Report	Full Text of Delegation	Link to Report containing the Delegation	Expiry date
23/09/2021	Project Appraisal: M27 Junction 10	That authority is delegated to the Director of Economy, Transport, and Environment, in consultation with the Head of Legal Services, to make the arrangements to complete the scheme design and Highways England's Approval processes; enter into highways agreements for dedication and adoption of any part of the M27 Junction 10 works and work on Highways England's asset; finalise the contractual documentation, including minor variations; and to implement the scheme.	PA - M27 Junction 10-2021-09-23-EMETE Decision Day (hants.gov.uk)	End of 2027
23/09/2021	Project Appraisal: M27 Junction 10	That the Director of Culture, Communities, and Business Services be requested to progress, if appropriate, any transfers of land identified in this report under the powers delegated by the Executive Member for Policy and Resources		End of 2027
13/07/2021	Report to Cabinet M27 Junction 10	That the value of the M27 J10 Improvement scheme in the County Council Capital Programme be increased from a value of £4.65m to £97.55 million, to be funded from Housing Infrastructure Grant and developer funding, and that authority be delegated to the Director of Economy, Transport and Environment, in consultation with the Leader, the Director of Corporate Resources and the Head of Legal Services to complete appropriate, aligned funding agreements;	Decision Record.pdf (hants.gov.uk)	End of 2027
13/07/2021	Report to Cabinet M27 Junction 10	That authority be delegated to the Director of Economy, Transport and Environment, in consultation with the Head of Legal Services to make and advertise necessary Road Orders and secure any additional statutory or land owner consents required; and		End of 2027
13/07/2021	Report to Cabinet M27 Junction 10	That authority be delegated to the Director of Economy, Transport and Environment, in consultation with the Leader, the Director of Corporate Resources and the Head of Legal Services to commence a staged procurement process, involving Early Contractor Involvement and main works contract and to spend up to £97.55 million, subject to confirmation of funding, following the completion of satisfactory financial agreements and approval of a scheme Project Appraisal by the Executive Lead Member for Economy, Transport and Environment.		End of 2027
15/01/2019	M27 Junction 10	That authority is delegated to the Director of Economy, Transport and Environment to pursue all potential external and partner funding and bidding opportunities to secure funding towards the Scheme development and delivery;	Appendix 1 (hants.gov.uk)	End of 2027
15/01/2019	M27 Junction 10	That authority be delegated to the Director of Economy, Transport and Environment and the Head of Legal Services to progress, enter into, and secure all appropriate licences, agreements, consents, rights, permissions and easements necessary to enable the advanced and main works to be undertaken on land owned by third parties, including Section 6 agreements to work on land owned by Highways England and licences to work on Buckland Estate land		End of 2027
15/01/2019	M27 Junction 10	That authority be delegated to the Director of Economy, Transport, and Environment and the Head of Legal Services to progress and make all Orders (including Side Road Orders) necessary to progress the Scheme towards delivery to ensure that the Orders can be advertised as soon as possible to enable timescales for sequential tasks to be achieved		End of 2027

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By virtue of paragraph(s) 3 of Part 1 of Schedule 12A
of the Local Government Act 1972.

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